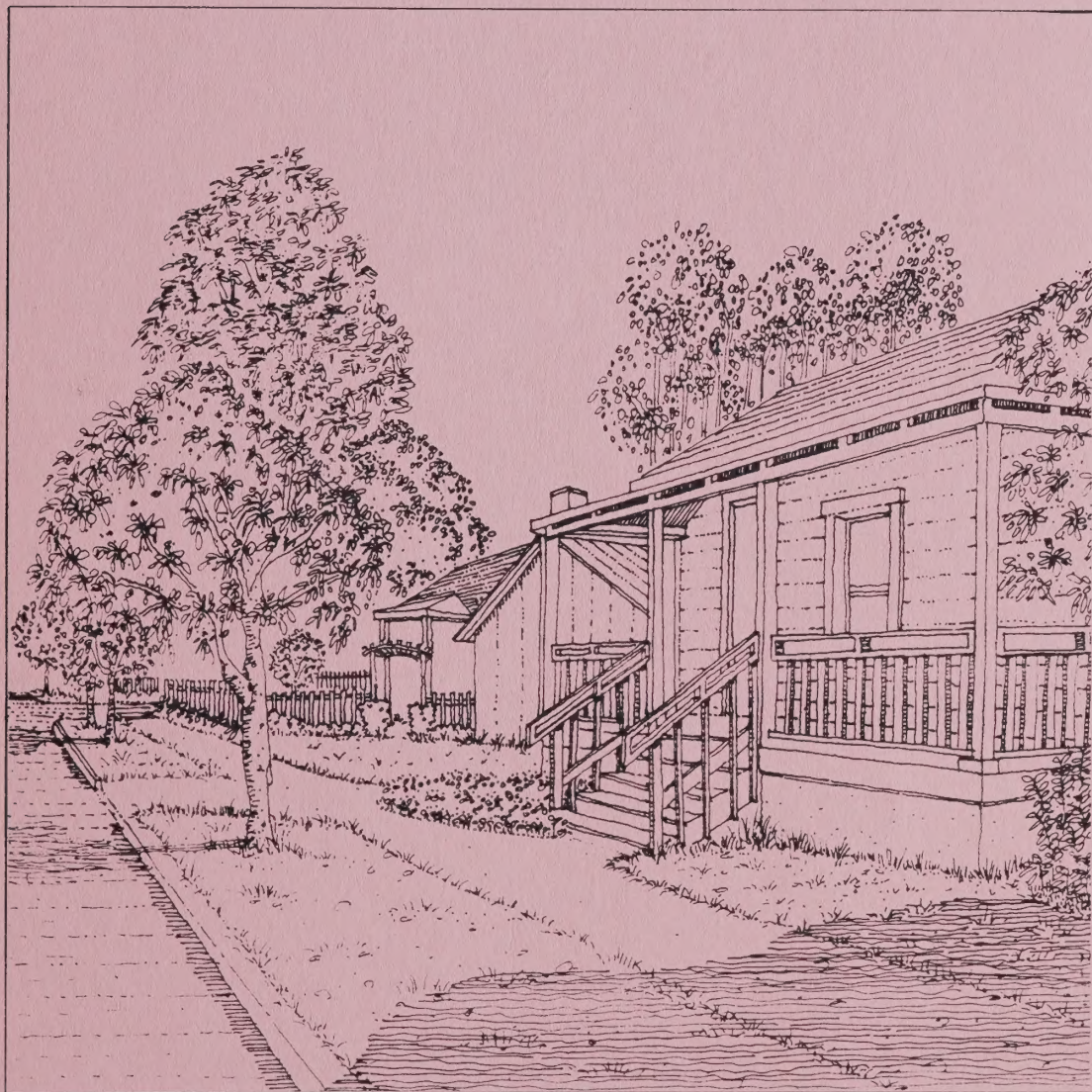


YOUNTVILLE DESIGN DOCUMENT

To Be Read In Conjunction With The Yountville General Plan And Zoning Ordinance

ORDINANCE 232-92 as Amended by ORDINANCE 242-94



Amended February 8, 1994

The following persons contributed to the 1994 Yountville Design Ordinance

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Special thanks to the unnamed land owners and residents
of Yountville who provided input during this amendment process.

T A B L E O F C O N T E N T S

INTRODUCTION	1-2
I. TOWN STRUCTURE	
A. Circulation	3-4
1. Street Design and Types	4-9
2. Parking and Driveways	10-12
3. Pedestrian and Bike Paths	12
B. Open Space & Parks	13-14
1. Open Space	14-18
2. View Corridors	19-20
3. Hopper Creek	20-21
4. Town Parks and Playfields	21
5. Trees, Landscaping and Maintenance	22-23
C. Individual Lots and Buildings	24
1. Configuration and Aggregation of Lots	24-25
2. Placement of Buildings and Parking	26
3. Yards, Setbacks and Encroachments	26-27
4. Walls, Fences and Landscape Screening	27-28
5. Utilities and Refuse Storage	28-29
D. Old Town Historical District	29-31
II. BUILDING TYPES & ARCHITECTURAL STANDARDS	
A. The Craft of Building	34
1. Building Materials	35
B. Relation of Buildings to Streets	35
1. Building Frontage and Orientation	36
2. Building Elements	36
C. Residential Buildings	36
1. Single-family Houses	37-39
2. Duplexes	40
3. Triplexes, Fourplexes and Apartments	41
4. Second Residential Units	42
5. Manufactured Housing	43
D. Commercial Buildings	44
1. Primary Commercial Buildings	44-45
2. Residential-scaled Commercial Buildings	46-47
3. Combined Commercial with Residential	48
E. Accessory Buildings and Facilities	49

III. SPECIFIC PARCELS AND USES

A.	Retained Uses	52
1.	APN 36-033-01 (Yountville Market)	53
2.	APN 36-033-15 (Pancha's Bar)	54
3.	APN 36-033-14 (McGrath's Garage)	55
4.	APN 36-090-05 (Napa Valley Express & Debbie's Beauty Salon)	56
5.	APN 36-040-11 (Knight Lumber Company)	57
6.	APN 36-053-04 (Webber Place)	58
7.	APN 36-051-03 (Bordeaux House)	59
8.	APN 36-051-05 (French Laundry)	60
9.	APN 36-061-15 (The Former U.S. Post Office)	61
10.	APN 36-440-01 (Burgundy House)	62
B.	Unbuilt Parcels	64-67
1.	APN 36-090-26 (Altamura)	64
2.	APN 36-090-22 & 23 (Bardessono)	64
3.	APN 36-081-10 (Byrnes)	65
4.	APN 36-081-08 (Carbonne)	65
5.	APN 36-090-02 (Catholic Church)	65
6.	APN 36-330-01 (Filippi)	65
7.	APN 36-040-12 & 14 (Forrester)	66
8.	APN 36-040-13 (Lande)	66
9.	APN 36-361-01 (Patton)	66
10.	APN 36-090-05 (Rabe)	66
11.	APN 36-330-07 (Vintage Partners)	67

ADOPTION	68-69
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IV. APPENDIX

A.	Case Studies for Unbuilt Parcels	70
1.	APN 36-090-26 (Altamura)	71-72
2.	APN 36-090-22 & 36-090-23 (Bardessono)	73-74
3.	APN 36-090-02 (Catholic Church) & APN 36-090-05 (Rabe)	75-76
4.	APN 36-330-01 (Filippi)	77-78
5.	APN 36-040-12 & 36-070-14 (Forrester) & APN 36-040-13 (Lande)	79-80
6.	APN 36-330-07 (Vintage Partners) & APN 36-081-10 (Byrnes) & APN 36-081-08 (Carbone)	81-82
B.	Cross Sections of Existing Streets	83
1.	Washington Street at Madison Street	83
2.	Washington Street near Yount Street	84
3.	Washington Street at Oak Circle	84
4.	Jefferson Street at Madison Street	85
5.	Yount Street at Madison Street	85
6.	Creek Street	86
7.	Vista Drive	86

F I G U R E S

&

T A B L E S

I.1	Town Map*	1
I.2	Street Identification Map*	3
I.3	Table of Existing Streets	5
I.4	Table of New Streets (with Designated Street Types)	6
I.5	Cross Section of Typical Residential Street, TRST	7
I.6	Cross Section of Narrow Residential Street, NRST	8
I.7	Cross Section of Residential Alley, RA	9
I.8	Possible Alternative Solutions to Parking Lots for Commercial Uses	10
I.9	Open Space/Parks Identification Map*	13
I.10	Conceptual Drawing 'A' of OpenSpace for APN 36-040-12 & 14 (Forrester)	15
I.11	Conceptual Drawing 'B' of OpenSpace for APN 36-040-12 & 14 (Forrester)	16
I.12	Map showing View Corridors	19
I.13	Existing Pattern of Old Town	24
I.14	Potential Pattern of New Residential Development	25
II.1	Sketch showing Building Envelope versus Building FAR	33
II.2a+b	Examples of Single-family Houses	37-39
II.3	Examples of Duplexes	40
II.4	Examples of Triplex and Fourplex Buildings	41
II.5	Examples of Second Units	42
II.6	Examples of Residential-scaled Buildings	46
II.7	Examples of Combined Commercial and Residential Buildings	48
III.1	Land Use Map* locating "Retained Uses"	51
III.2	Map of Unbuilt Parcels*	63
IV.1	Building Types for Case Studies	70
IV.2	Case Study APN 36-090-26 (Altamura)	72
IV.3	Case Study APN 36-090-22 & 36-090-23 (Bardessono)	74
IV.4	Case Study APN 36-090-02 (Catholic Church) & APN 36-090-05 (Rabe)	76
IV.5	Case Study APN 36-330-01 (Filippi)	78
IV.6	Case Study APN 36-040-12 & 14 (Forrester) & APN 36-040-13 (Lande)	80
IV.7	Case Study APN 36-330-07 (Vintage Partners) & APN 36-081-10 (Byrnes) & APN 36-081-08 (Carbone)	82
IV.8	Cross Section of Washington Street at Madison Street	83
IV.9	Cross Section of Washington Street near Yount Street	84
IV.10	Cross Section of Washington Street at Oak Circle	84
IV.11	Cross Section of Jefferson Street at Madison Street	85
IV.12	Cross Section of Yount Street at Madison Street	85
IV.13	Cross Section of Creek Street	86
IV.14	Cross Section of Vista Drive	86

*Larger scale maps available upon request from the Town Clerk

Design Document
Town Structure



Figure I.1

Town Map

I N T R O D U C T I O N

The Town concurrently adopted the 1992 revision of the General Plan and the Zoning Ordinance, and a new Design Document. The creation of the Yountville Design Document represents a critical step in the town's history. Of the three documents, it has the most direct influence on the physical aspects of Yountville. The Design Document outlines detailed standards for specific components of development related to the Town Structure and building types. These include regulations for street width, parking, driveways, walls, fences, landscape, refuse, utilities, FAR, building height, yards and setbacks or similar quantifiable components of design. The procedures to enforce the Design Document are outlined in the Zoning Ordinance which spells out the criteria and conditions necessary to accomplish the intent of the General Plan. The 1992 General Plan establishes the conceptual basis for issues, objectives and policies which face the town such as providing adequate services, affordable housing, and maintaining a viable business community consistent with its small town rural character.

If it occurs that the Design Ordinance and Zoning Ordinance address similar issues in different ways, then the Zoning Ordinance will prevail in areas of lot size, process permits, permitted uses, use criteria and findings, general conditions and use restrictions. The Design Ordinance shall prevail in all aspects of the design of physical improvements, including but not limited to FAR, density, setbacks, height, width, landscaping open space areas, materials, architectural elements, and lot design.

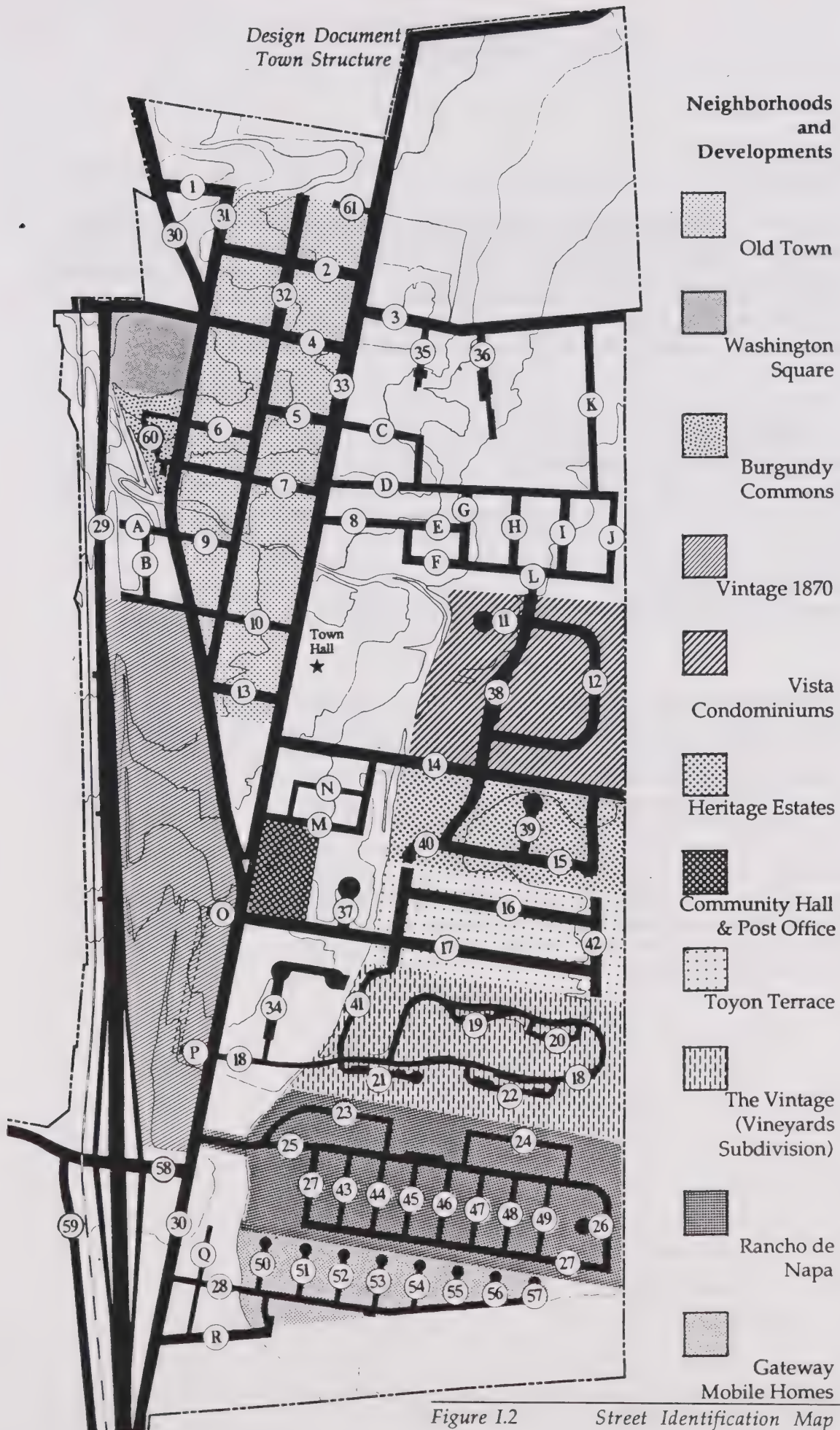


Figure I.2

Street Identification Map

CHAPTER I.

TOWN STRUCTURE

The General Plan discusses the importance of the Town Map and its role in shaping growth. The components of the Town Map are circulation, open space, parks, and land use. While circulation, specifically a street grid, serves as the primary organizing element of the town, it is the interconnectedness of open space, parks, and land use which will reinforce the walkable aspects of Yountville. This chapter contains design standards which regulate the essential components of the Town Structure.

A.

CIRCULATION

The General Plan discusses the basis for the layout of streets according to gridiron planning principles, and establishes the intended location of new streets. The intent of the street grid is to provide multiple routes which serve to distribute traffic as well as make the town a cohesive whole. In a few places streets terminate in T-intersections with cross streets to slow traffic and discourage excessive through traffic on any one street. This document outlines standards for streets which define their spatial character and relationship to buildings. These include the width of streets, provisions for street trees, and accommodations for parking and pedestrians.

A.1.

Street Design and Types

An important element of the rural character of Yountville is the design of its streets. In the older sections of town the pastoral feel of an agrarian community is established by trees which narrow the street space, and by gravel shoulders without curbs, gutters or sidewalks. In some newer sections there is a decidedly suburban feel created by a lack of street trees, and conventional concrete curbs, gutters and sidewalks. This Design Document has two intentions with respect to street design: first, to establish design standards for future development that perpetuate the rural rather than the suburban aspects of Yountville; and second, to establish design standards for improvements on existing streets. It is through a capital improvements program that the rural character of streets can be made consistent through most of the town. For reference, cross sections of several existing streets have been included in this document, and are illustrated in Chapter IV, The Appendix.

A.1.a

Design Standards for Streets

The Street Identification Map, shown on the previous page in Figure I.2, locates all new streets and establishes locations/alignments, extensions and through-connections between new and existing streets. The proposed new streets are identified by letter, and listed below in Figure I.3. Significant changes in the street configuration are not permitted, however, there are three degrees of flexibility for the location of the proposed new streets. In cases where new streets are extensions of existing streets for the purpose of preserving view corridors, the streets are fixed and alteration of their location/alignment would not comply with the intent of the General Plan. In other cases where the location of new streets is important to reinforce the Town Structure, the streets may shift five to ten feet as a result of topographic, or other natural features of a given site. And in a few cases, the location of the new streets or alleys may shift up to 25 feet. Changes will be permitted only if they do not adversely alter the intent of the gridiron planning principles established in the General Plan.

EXISTING STREET NAME	EXISTING STREET NAME	EXISTING STREET NAME
1 Jackson Street	21 Vintner Court, private	41 Heather Street
2 Monroe Street	22 Colombard Way, private	42 Holly Street
3 Yountville Cross Road	23 San Carlos, private	43 San Juan Capistran, private
4 Madison Street	24 San Miguel, private	44 San Domingo, private
5 Adams Street	25 Mission, private	45 San Fernando, private
6 Pedroni Street	26 Mission Court, private	46 San Francisco, private
7 Starkey Avenue	27 San Antonio, private	47 San Diego, private
8 Mount Avenue	28 Champagne Drive, private	48 Santa Barbara, private
9 Creek Street	29 California State Hwy. 29	49 Santa Cruz, private
10 Webber Avenue	30 Washington Street	50 Burgundy Circle, private
11 Vista Court	31 Lincoln Avenue	51 Chablis Circle, private
12 Vineyard Circle	32 Jefferson Street	52 Gamay Circle, private
13 Humboldt Street	33 Yount Street	53 Port Circle, private
14 Finnell Road	34 Oak Leaf Court	54 Riesling Circle, private
15 Heritage Way	35 Mesa Court	55 Sherry Circle, private
16 Larkspur	36 Tallent Lane, private	56 Sauterne Circle, private
17 Mulberry Street	37 Ivy Court	57 Vin Rose, private
18 Oak Circle	38 Vista Drive	58 California Drive
19 Hopper Way, private	39 Harvest Court	59 Solano Avenue
20 Carignan Way, private	40 Heritage Court	60 Burgundy Way, private
		61 Grant Lane, private

Figure I.3

Table of Existing Streets

Design Ordinance
Town Structure

The table below designates the street type for each new street, and is keyed to a definitive description of each street type including design standards. The degree of flexibility is also indicated for each proposed new street. Residential alleys have been located within parcels which have frontage on Washington Street to minimize the amount of potential curb cut, and enhance the flow of traffic. In all cases, the distance between new buildings facing each other on new residential streets and alleys shall be at least 30 feet.

The design standards establish specific dimensions and definitive characteristics for the three following types of public rights-of-way: 1) typical residential street or TRST; 2) narrow residential street or NRST, and 3) residential alley or RA. For all street types, there shall be gravel shoulders flanking both sides of the paved area. However, to ensure proper drainage, there shall be adequate swale provided within the gravel shoulders, and in some cases, a concrete v-ditch no more than two feet wide may be placed within the shoulder. Where deemed appropriate the Town may require a sidewalk on one side of typical residential streets.

LETTER	STREET NAME	STREET TYPE	DEGREE OF FLEXIBILITY
A	Extension of Creek Street	NRST	Fixed
B	New Street, Filippi	NRST	Flexible up to 25 feet
C	Extension of Adams Street	NRST	Fixed
D	New Street, Lande/ Forrester	NRST or TRST	Flexible up to 25 feet
E	Extension of Mount Avenue	NRST	Fixed
F	New Street, Forrester	NRST	Flexible up to 10 feet
G	New Street, Forrester	RA	Flexible up to 25 feet
H	New Street, Forrester	RA	Flexible up to 25 feet
I	New Street, Forrester	RA	Flexible up to 25 feet
J	New Street, Forrester	RA	Flexible up to 25 feet
K	Extension of Stags View Lane	NRST	Flexible up to 10 feet
L	Extension of Vista Drive	TRST	Fixed
M	New Street, Bardessono	NRST	Flexible up to 10 feet
N	New Street, Bardessono	RA	Flexible up to 25 feet
O	Extension of Mulberry Street	NRST or TRST	Fixed
P	Extension of Oak Circle	NRST or TRST	Fixed
Q	New Street, Rabe	RA	Flexible up to 10 feet
R	New Street, Catholic Church	NRST or TRST	Flexible up to 10 feet

Figure 1.4

Table of New Streets

TYPICAL RESIDENTIAL STREET, TRST

The cross section for a typical two-way residential street right-of-way includes a paved area to accommodate two travel lanes flanked by parallel on-street parking, and gravel shoulders to provide for drainage, paths for pedestrians, and street trees. The following dimensions and conditions apply to a street of this type:

- 1 The public right-of-way shall not exceed 52 feet in width;
- 2 The width of paved area shall not exceed 36 feet, and consist of two 10-foot lanes flanked by two eight-foot parking lanes;
- 3 The width of each gravel shoulder shall not exceed eight feet; and
- 4 Trees shall be planted at the edge of the street right-of-way, at a spacing of no less than one tree every 30 feet.

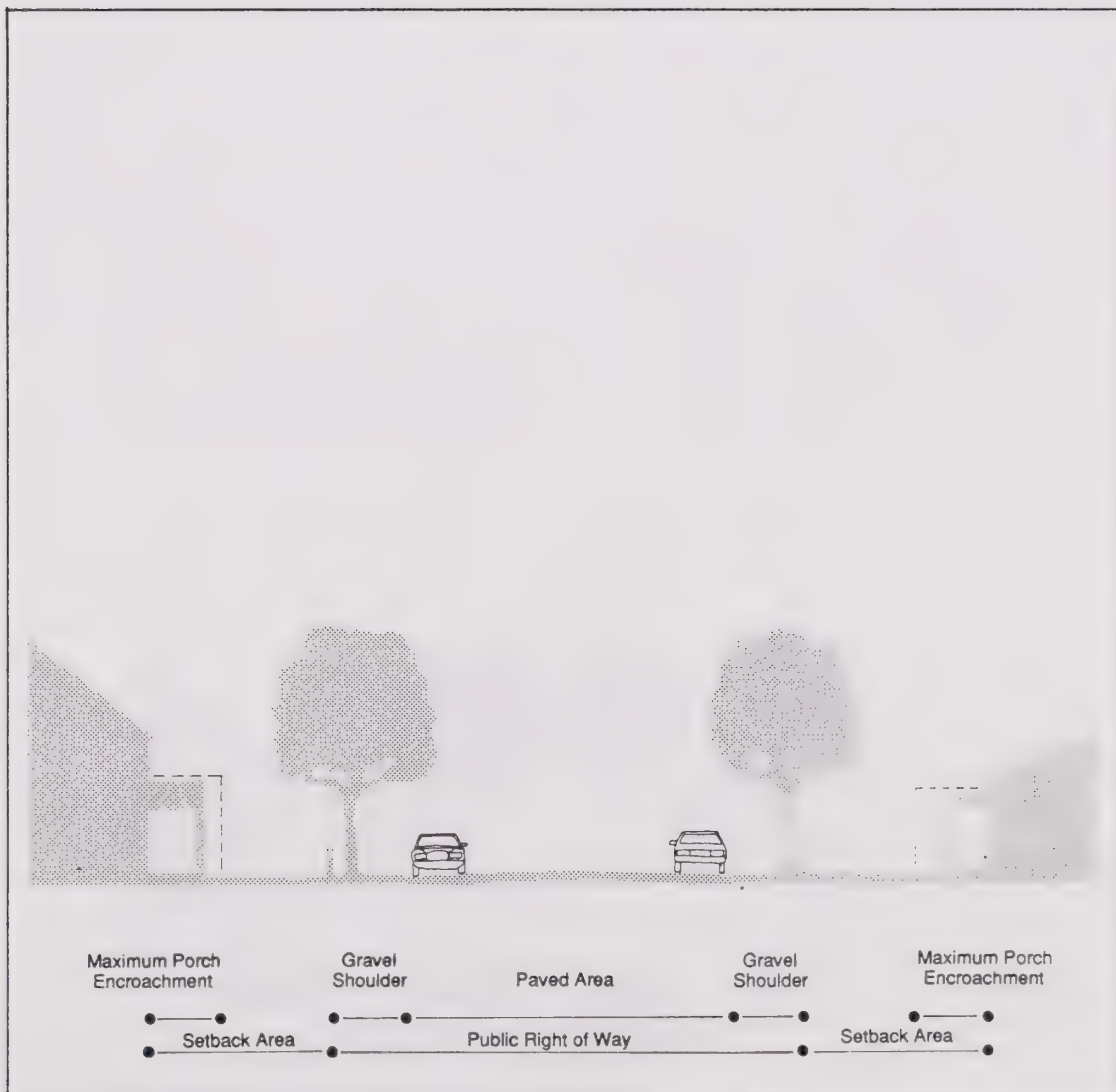


Figure I.5

Typical Residential Street, TRST

NARROW RESIDENTIAL STREET, NRST

The cross section for a narrow two-way residential street right-of-way includes a paved area to accommodate two travel lanes, and gravel shoulders parallel to the paved area to provide for drainage, paths for pedestrians, and street trees. The following dimensions and conditions apply to this street type:

- 1 The public right-of-way shall not exceed 40 feet in width;
- 2 The width of paved area shall not exceed 24 feet, and consist of two 12-foot lanes;
- 3 The width of each gravel shoulder shall not exceed eight feet; and
- 4 Trees shall be planted at the edge of the street right-of-way, at a spacing of no less than one tree every 30 feet.

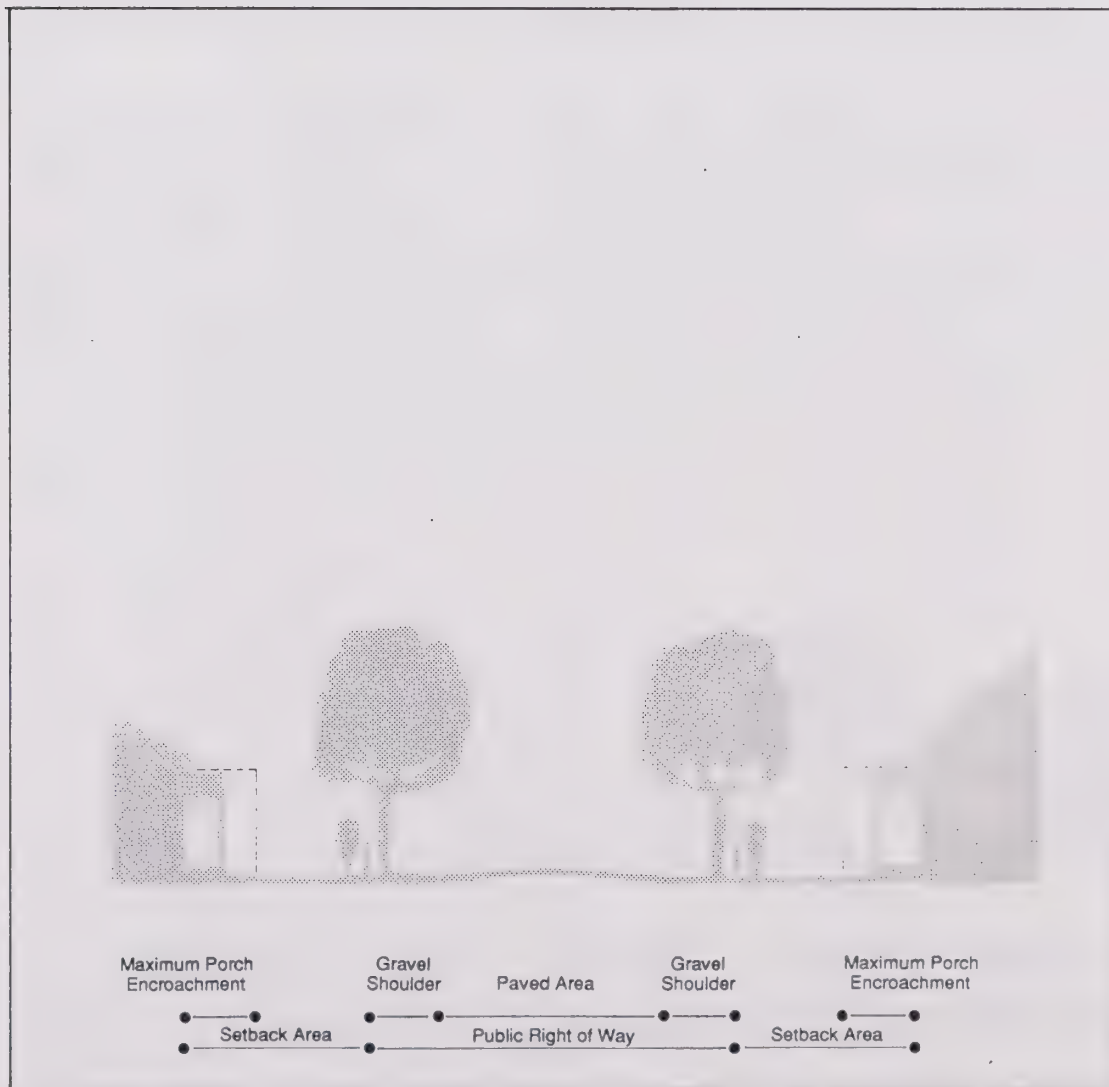


Figure I.6

Narrow Residential Street, NRST

RESIDENTIAL ALLEY

The cross section for a residential alley right-of-way includes a paved area to accommodate one or two travel lanes, gravel shoulders parallel to the paved area to provide for drainage, paths for pedestrians, and street trees. The following dimensions and conditions apply to residential alleys:

- 1 The public right-of-way for an alley shall be at least 21 feet in width for a one-way alley, and not exceed 26 feet for a two-way alley;
- 2 The width of paved area shall not exceed 20 feet for two-way, and 15 feet for one-way;
- 3 The width of each gravel shoulder shall not exceed three feet; and
- 4 Trees are encouraged to be planted along the edge of the right-of-way, at a spacing of no less than one tree every 30 feet.

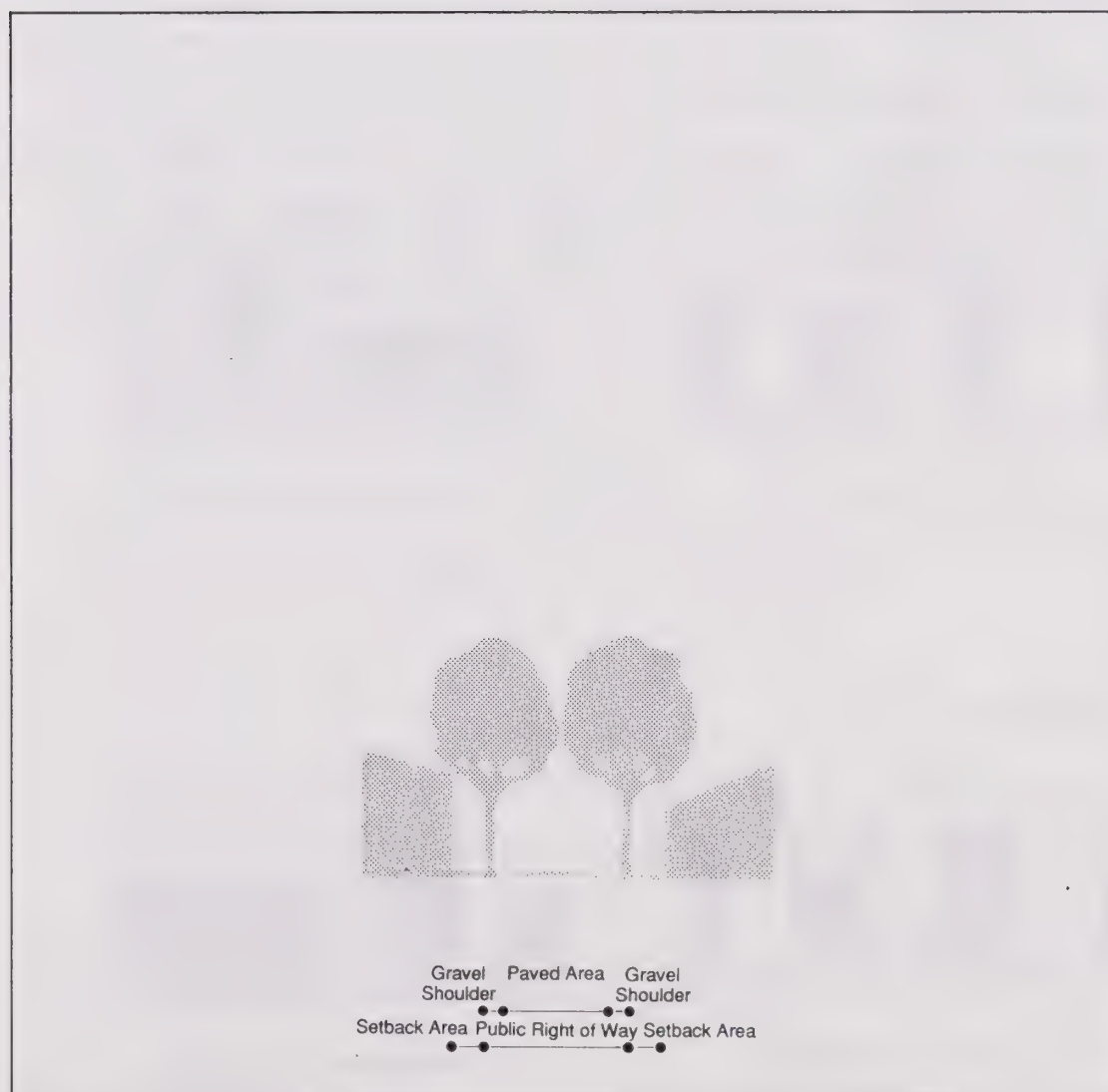


Figure I.7

Residential Alley, RA

A.2.

Parking and Driveways

In many parts of California, streets dominated by driveways, garages and garage doors have eroded the sense of community that characterizes older towns. Therefore, parking and driveways are an important component for Yountville. In most parts of town, parked cars and driveways are handled with grace and discretion. On many streets, cars move slowly and streetscapes are enlivened by buildings, their windows, porches and entrances. In the past, parking was placed in behind buildings, or in the rear of the lot. Driveways were modest in width but adequate. Curb cuts were few and small. Today however, there are a few places where parking lots and curb cuts have usurped the street frontage, and destroyed the coherence of the street. Placement of parking should minimize or conceal large areas of paving and garage doors. The design standards are created to accommodate parking and driveways in ways which minimize their impact on Yountville's neighborhoods. The drawings below show different possible configurations and locations of parking which reinforce the continuity of the street frontage, particularly for streets serving commercial uses. The intent is to minimize the impact of parking while providing adequate parking for all uses.

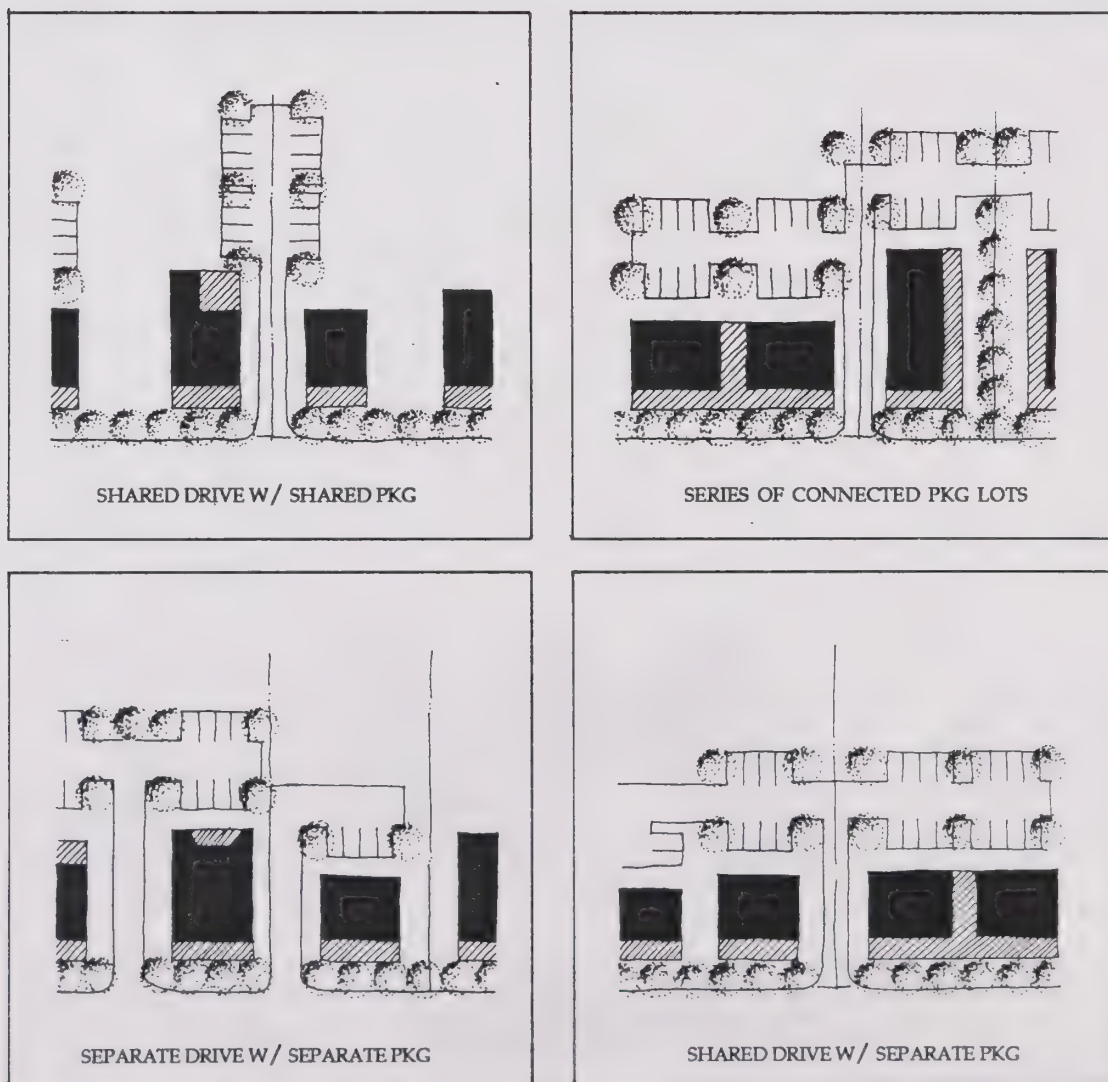


Figure I.8 Possible Alternative Solutions to Parking Lots for Commercial Uses

A . 2 . a

Design Standards For Parking

The following design standards shall apply to new development throughout the town:

- 1 Parking ratios and stall dimensions shall be as established in Section 6.2 of the Zoning Ordinance;
- 2 Parking shall be located behind buildings or in the rear of the lot. Exceptions may be granted as a result of the shallow depth of a parcel, existing large healthy trees, or similar circumstance;
- 3 Large contiguous areas of impervious paving are discouraged. Where possible, unit pavers, brick, gravel, or other permeable materials should be used;
- 4 Parking lots shall be screened along all street frontage with walls or evergreen landscaping at least three feet in height. Along common property lines and abutting residential uses, walls and landscape screening shall be provided as established in Chapter I, Section C.4;
- 5 Within parking areas, there shall be one tree provided for every six parking spaces. (Six spaces may be configured in different ways, for example, three in a row facing each other or six in a single line.) Trees shall be planted in tree wells of at least four feet wide by four feet long by four feet deep and adequately protected from car movements. All landscaping and trees shall be provided with an irrigation system which is maintained in working order;
- 6 Bumpers, posts, wheel stops, or other similar devices shall be provided on all parking spaces located along property lines, and set back a minimum of eighteen inches from the boundary of the parking lot; and
- 7 Lighting for parking areas shall be designed to confine emitted light to parking areas, and the light source shall not be visible from outside the area. Glare or shine from lighting shall not create a nuisance for adjacent dwelling units. Average illumination at the ground shall be no more than one foot candle, except where an increase in lighting level is recommended by a lighting consultant or qualified professional as necessary for safety.

A . 2 . b

Design Standards for Driveways

The following design standards shall apply to new development throughout the town:

- 1 On residential properties, driveways for single-lot-access shall not exceed 10 feet in width, and driveways providing shared access shall not exceed 12 feet in width;
- 2 On residential properties, curb cuts for single car access shall not exceed 12 feet in width, and where possible, the minimum distance between curb cuts should be at least 20 feet;
- 3 Curb cuts for access to parking lots shall not exceed 24 feet in width for two-way access and 12 feet in width for one-way access. Exceptions may be granted by the Town Council as deemed necessary for public safety or preservation of existing landscape;
- 4 The pattern of circulation, including access drives and pedestrian paths, should provide easy access from the parking lot to residential or commercial uses;
- 5 Access drives to off-street parking shall be designed and constructed to provide adequate safety for pedestrians and drivers. In no case shall car movements result in blocking of the street right-of-way. The number of access drives shall be limited to the minimum that will accommodate anticipated traffic;
- 6 To minimize the amount of paved area throughout the town, the sharing of driveways and access to parking lots is encouraged. An easement providing for shared use shall be recorded;
- 7 For shared driveways exceeding 100 feet in length, turnouts may be needed for vehicles to pass one another;
- 8 The width of each garage door shall not exceed 12 feet when located in the front half of the lot and visible from the street;
- 9 Encroachments into a driveway area shall be limited to roof overhangs, projecting eaves, awnings, second story bay windows and similar building elements; and
- 10 Where practical, paving should be of a permeable material such as gravel, grass-concrete or similar unit pavers.

A . 3 .

Pedestrian and Bike Paths

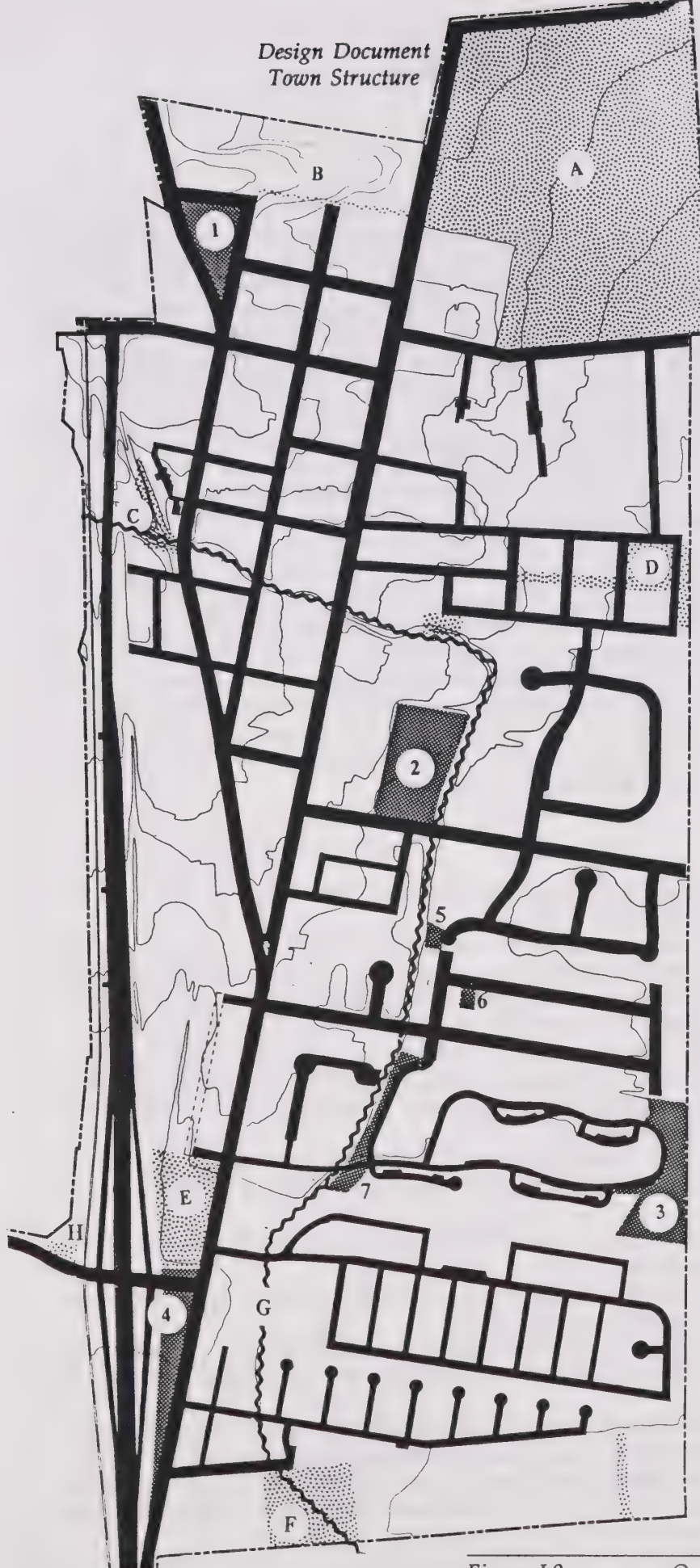
In Yountville, walking is a pleasant, and practical way to get from place to place. Some streets have concrete sidewalks, and others gravel shoulders. For a more leisurely walk there is a path along most of Hopper Creek which is shared by pedestrians and a few bicyclists. There are several places where the street network and this path cross throughout town. Most bicyclists ride through town along Washington Street and Yountville Cross Road where there are extra-wide paved shoulders.

A . 3 . a

Design Standards

The following design standards shall apply to new development throughout the town:

- 1 Gravel shoulders on all new streets shall be provided to serve as pedestrian paths while taking into account the safety of school children. New concrete sidewalks are discouraged. However, hard surface paving may be provided when needed to comply with accessibility standards for the disabled; and
- 2 Design standards for the path along Hopper Creek are established in Section B.3 and regulations regarding the setback area are established in Section 5.3 of the Zoning Ordinance.



TOWN PARKS

1. Yountville Park
2. School Playfield
3. The Vinyards Park
4. Veteran's Memorial Park
5. Heritage Estates Park
6. Toyon Terrace Tot-Lot
7. Hopper Creek Park



OPEN SPACE

- A. Pelissa Property
- B. Trail @ Cemetery
- C. Portion of APN 36-330-01 (Fillipi)
- D. Portion of APN 36-070-14 (Forrester)
- E. Portion of APN 36-330-07 (Vintage Partners)
- F. Portion of APN 36-090-02 (Catholic Church)
- G. Hopper Creek
- H. Open Space

Figure I.9

Open Space/Parks Map

B .

OPEN SPACE & PARKS

Open space is an encompassing category of land use which includes areas for outdoor leisure and recreation, as well as, the preservation of natural resources. The General Plan designates areas within parcels for use as open space to serve the surrounding residents, and provide an amenity to the town. The Zoning Ordinance defines the various terms which distinguish between usable versus residual, and private versus common open space. This document establishes the intent and design standards for open space within specific parcels and throughout the town in ways which reinforce the Town Structure. The components of the open space network include preserved agricultural uses, neighborhood parks, view corridors, town parks and playfields, and Hopper Creek. In some cases, open space is land which is accessible to the public, although it is privately owned and maintained.

B . 1 .

Open Space

The General Plan identifies existing parks and other open space areas throughout town and establishes new areas within a few unbuilt parcels for use as open space. These are shown in Figure I.8, Open Space/Parks Map, to the left by letter, and include APN 36-330-01 (Filippi), APN 36-330-07 (Vintage Partners), APN 36-090-02 (Catholic Church), and APN 36-040-12 & 14 (Forrester). Design standards pertaining to the proposed open space within each parcel are outlined below. Specific requirements for individual lots and buildings are listed per building type in Chapter II.

B . 1 . a

Design Standards

The following design standards apply to open space throughout town as shown on the left in Figure I.8, Open Space/Parks Map:

- 1 Yards or other open space required by this document, the Zoning Ordinance, or the General Plan shall not be considered as providing a yard or open space for any other lot, building or structure, unless proposed and approved as a part of a Master Development Plan as established in Section 7.4 of the Zoning Ordinance;
- 2 Recreational structures, such as swing sets and jungle gyms, designed to be consistent with the intent of the open space requirements will not be considered accessory buildings, but shall be considered usable open space, and therefore, not included in the tabulation of FAR or lot coverage;
- 3 For Common Usable Open Space: Required area shall be permanently maintained and remain fully usable. Landscaping and maintenance shall be as established in Chapter I, Section B.5;
- 4 In residential zoning districts common usable open space areas shall have a minimum dimension of 15 feet in all directions.
- 5 For Common Usable Open Space: Areas shall not be screened so as to restrict the continuity of, or visual access to the open space;
- 6 For Common Usable Open Space: Decks may be credited towards the total area requirements. Decks shall be functional, structurally safe, adequately surfaced and protected;
- 7 For Private Usable Open Space: Areas shall be at least 100 square feet, shall have a minimum dimension of five feet in all directions and not be a passageway; and
- 8 For Private Usable Open Space: A wall, fence or landscape screening may be required to provide adequate privacy for private open space, and shall be subject to height limits as established in Section C.4 of this chapter.

For APN 36-040-12 & 14 (Forrester):

The central portion of the Forrester parcel shown as open space is intended to provide a pedestrian and visual link to Old Town from the new residential neighborhood, and extend the existing view corridor on Mount Avenue. The Open Space/Parks Map, Figure I.8, shows a series of linear open spaces within the new blocks culminating in a larger square encompassing the existing farmhouse and large trees near the existing house, and a linear portion along Beard Ditch on the eastern boundary. Portions of open space shown around the existing farmhouse and along Beard Ditch are intended to preserve the existing large healthy trees, and provide new view corridors for new streets D & F. All of the open space may also serve as a neighborhood park for nearby residents.

The conceptual drawings illustrate two possible solutions which meet the intent. In Drawing 'A' below, there is a double row of trees defining a series of linear parks terminated by a perpendicular linear park edged by the existing large trees along Beard Ditch. In Drawing 'B' on the opposite page, the drawing shows potential alleys extending Mount Avenue, and along the eastern boundary shows shallow individual lots between the new and existing view corridors. The total area shown is between one and one plus a third acres, however, the ultimate configuration and size of the open space area depends on the layout of lots and natural features of the site.

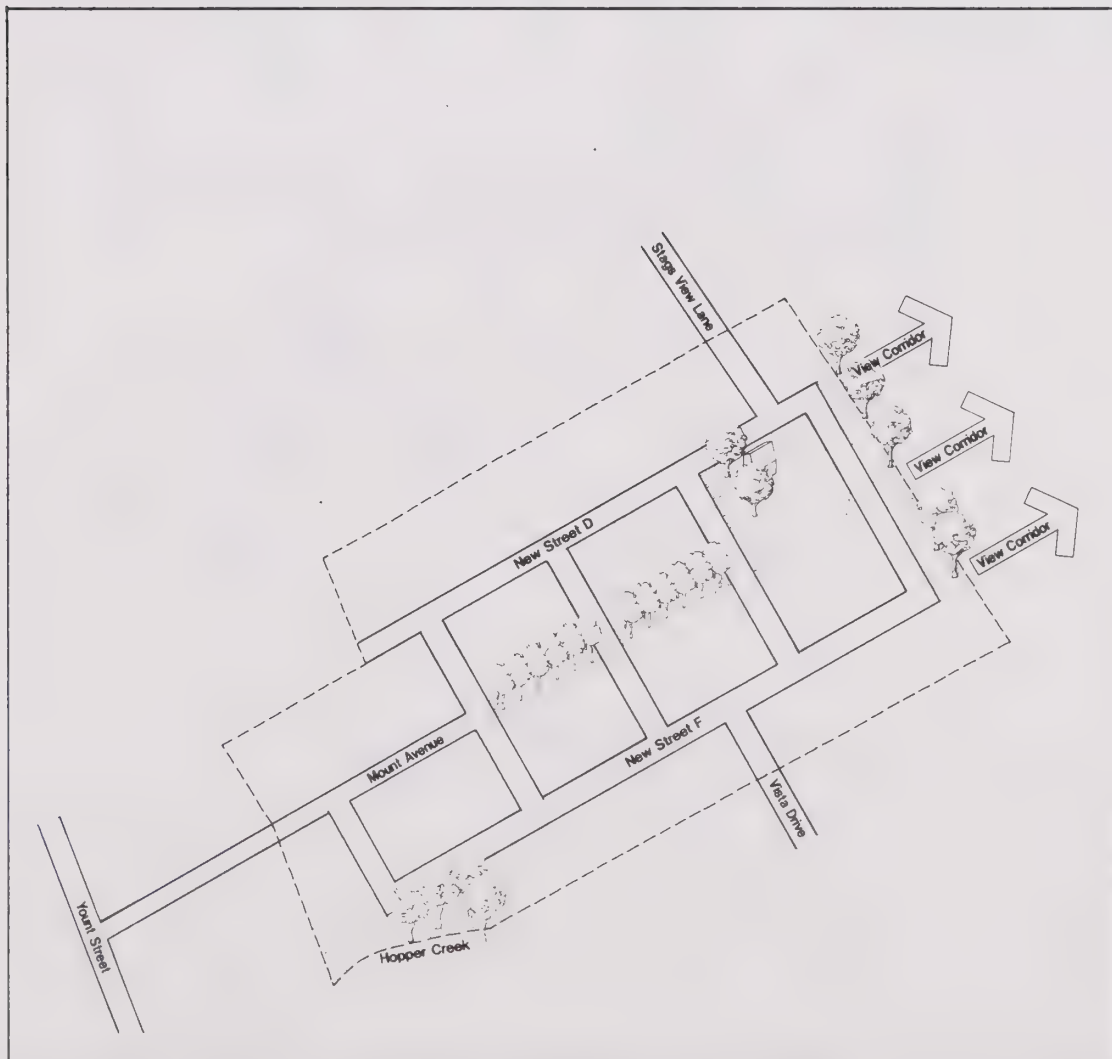


Figure I.10

Conceptual Drawing 'A' of Open Space for Forrester property

The following applies to the open space area within APN 36-040-12 & 14 (Forrester):

- 1 The width of the open space which defines existing and new view corridors shall be at least 35 feet clear of any obstruction by buildings or structures, and 20 feet clear of any obstruction by trees or landscaping;
- 2 The open space shall be distributed through out the property to preserve the existing large healthy trees and preserve the area along Hopper Creek;
- 3 To ensure the visual connection of the new neighborhood to the surrounding landscape, new view corridors shall be established for new streets D & F along the eastern boundary;
- 4 The Mount Avenue view corridor shall be maintained;
- 5 All plantings shall be low-maintenance and drought-tolerant. Large areas of lawn are discouraged. Recommended plantings include evergreen groundcovers and seasonal wildflowers;
- 6 Occasional benches are encouraged, and when provided, shall be made of wood;
- 7 Where lighting is provided, low bollards are preferred. In the event lamp posts are used, they shall not exceed 10 feet high; and
- 8 Walls, fences, or landscape screening which are contiguous with the designated open space shall not exceed three feet in height.

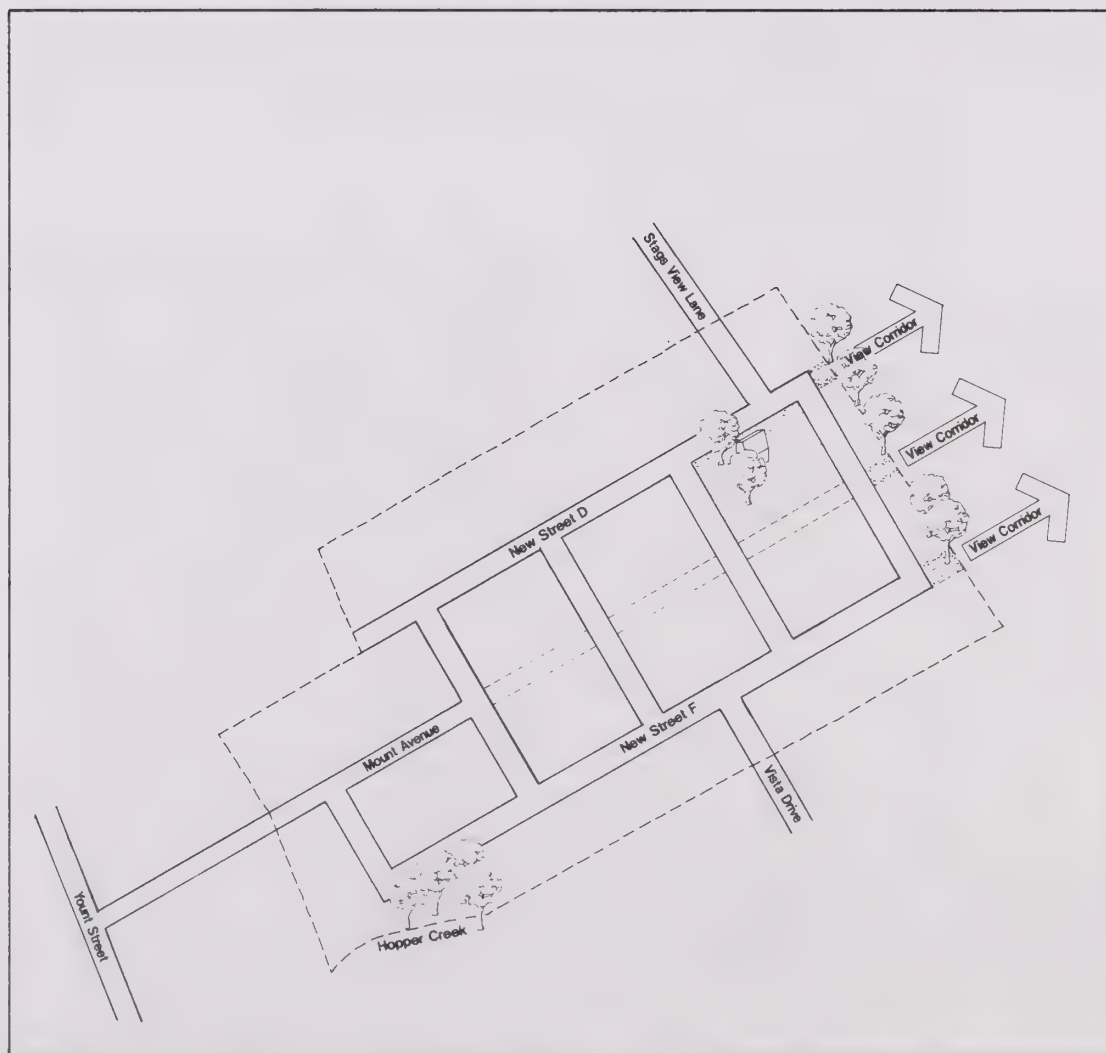


Figure I.11

Conceptual Drawing 'B' of Open Space for Forrester property

For APN 36-330-01 (Filippi):

The portion of the Filippi parcel shown as open space is intended to enhance the natural character of Hopper Creek. Primarily located on the northside of Hopper Creek, the triangular-shaped area is about half an acre of land and is inaccessible by auto. In addition to design standards for all open space outlined in B.1.a 1-7, the following criteria apply to the area shown as open space in Figure I.8:

- 1 There shall be a continuous path which begins at Washington Street continues along the north side of the creek to the parcel's western boundary (the CalTrans right-of-way), turns south, and connects with a pedestrian bridge the existing path at the north end of Vintage 1870 property at the Vintage Inn;
- 2 When changes to the existing landscape are under review, the frontage along Highway 29 shall be consistent with intent of the General Plan regarding view corridors and Hopper Creek, and Section B.2 and B.3, respectively, of this chapter;
- 3 New plantings should be similar to existing native vegetation;
- 4 Existing large healthy trees and other significant landscaping shall be retained as outlined in Section B.5 of this chapter;
- 5 In the event lighting is provided, bollard lighting is preferred; and
- 6 Benches are encouraged, and when provided, shall be made of wood.

For APN 36-330-07 (Vintage Partners):

The portion of the Vintage Partners parcel shown as open space is intended to maintain the rural feeling at the southern entrance to town. The delineated area is about two acres south of Oak Circle between Washington Street and State Highway 29. The following criteria apply to the area shown as open space in Figure I.8 to the left:

- 1 Any use of the open space shall be agricultural in nature and no buildings, parking or permanent storage shall be permitted;
- 2 When changes to the existing landscape are under review, the frontage along Highway 29 shall be consistent with intent of the General Plan regarding view corridors and Section B.2 of this chapter;
- 3 In the event lighting is provided, bollard lighting is preferred; and
- 4 Benches are encouraged, and when provided, shall be made of wood.

For APN 36-090-02 (Catholic Church):

The portion of the Catholic Church parcel shown as open space is intended to maintain and enhance the natural character of Hopper Creek. Flanking Hopper Creek there is a square-shaped area of about three acres of land along the eastern boundary of the property. The open space may also serve church-related activities and as a neighborhood park for residents. The following criteria apply to the area shown as open space in Figure I.8:

- 1 There shall be a continuous path which connects to the path along the northern boundary on the property APN 36-330-01 (Rabe) and continues to the southern boundary at the town limits;
- 2 New plantings within the Hopper Creek setback area should be similar to existing native vegetation, other plantings shall be consistent with regional landscaping;
- 3 In the event lighting is provided, bollard lighting is preferred; and
- 4 Benches are encouraged, and when provided, shall be made of wood.

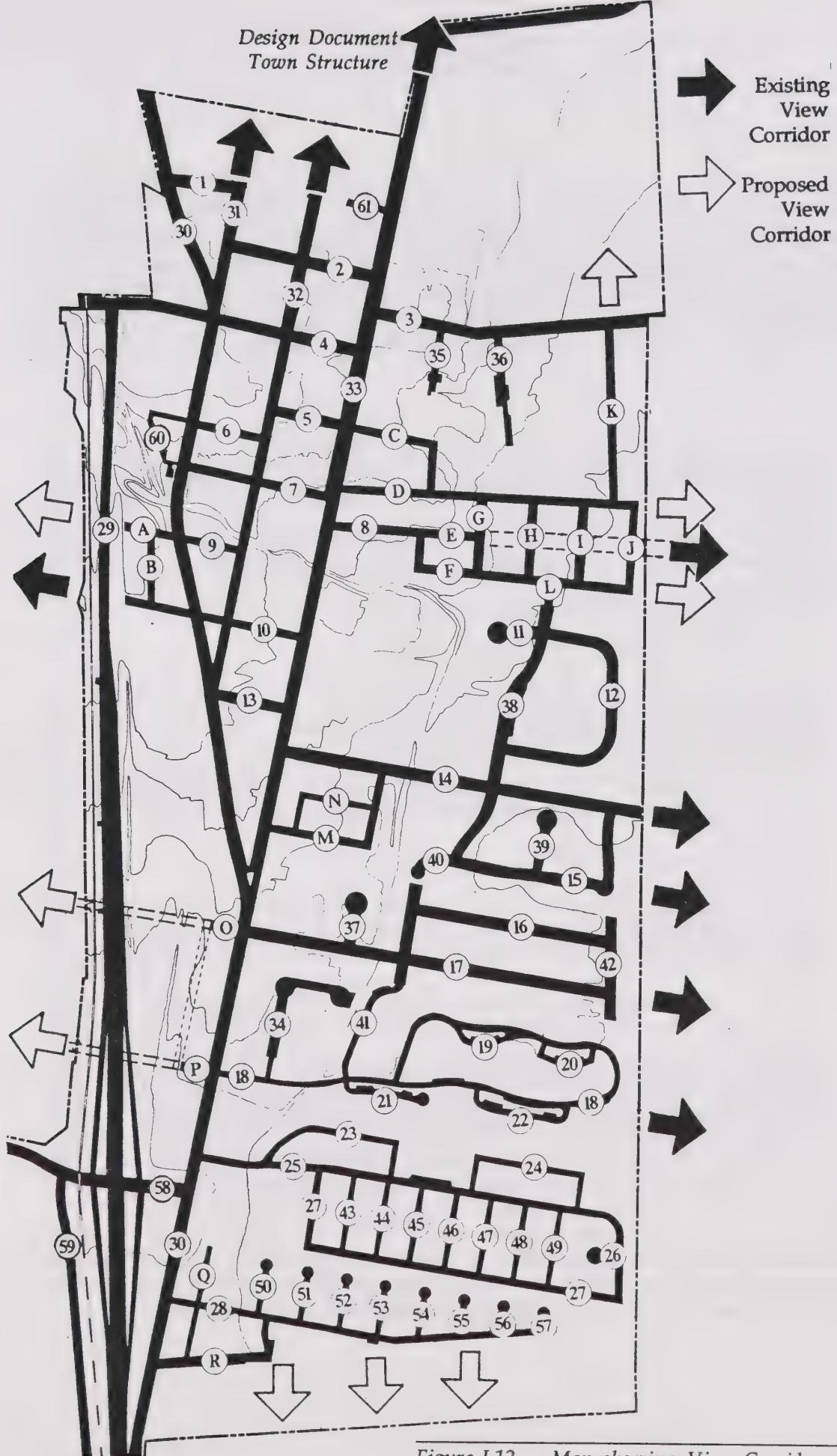


Figure I.12 Map showing View Corridors

B . 2 .

V i e w C o r r i d o r s

The General Plan discusses the importance of view corridors to the Town Structure of Yountville, and identifies proposed new view corridors. The intent of all view corridors is to provide a visual connection from places within the town to the surrounding landscape. All view corridors throughout the town are shown in Figure I.11, on the opposite page, and listed below with applicable design standards.

B . 2 . a

D e s i g n S t a n d a r d s

The following design standards apply to view corridors throughout town:

- 1 The width of open space which defines existing and new view corridors shall be at least 35 feet clear of any obstruction by buildings or structures, and 20 feet clear of obstruction by trees and landscaping;
- 2 Landscaping and maintenance standards are as established in Section B.5;
- 3 The following new streets shall be established as new view corridors, and maintained as such forever:
 - a New View Corridor A Extension of Creek Street;
 - b New View Corridor D New Street Forrester;
 - c New View Corridor E Extension of Mount Avenue;
 - d New View Corridor F New Street Forrester;
 - e New View Corridor K Stags View Lane;
 - f New View Corridor O Extension of Mulberry Street;
 - g New View Corridor P Extension of Oak Circle;
 - h New View Corridors North-South Streets within APN (Catholic Church);
- 4 The following streets currently serve as view corridors, and shall be maintained as such forever:
 - a Webber Street;
 - b Yount Street;
 - c Jefferson Street;
 - d Washington Street;
 - e Finnell Road;

B . 3 .

H o p p e r C r e e k

The importance of Hopper Creek as a natural resource, and the maintenance of its banks and existing vegetation is acknowledged in Chapter IV of the General Plan. The boundaries of the setback area along Hopper Creek are as established Section 5.3 of the Zoning Ordinance. There is also a path which follows the bank of the creek throughout most of town for the enjoyment of the creek by pedestrian and bicyclists. However, there are a few discontinuities where unbuilt parcels or development which existed prior to the path adjoin Hopper Creek. Since the overall intent of the path is to reinforce the Town Structure and provide benefit to the town and its residents, alternative routes should be considered to ensure continuity of the path. Where it is infeasible to connect the path as a result of existing development, alternative locations should be considered with the possibility of routing the path to local streets.

Design standards and layout for the path are outlined in the Yountville Bike Plan. The specific unbuilt parcels where the path remains to be provided include the following: 1) APN 36-090-22 & 23 (Bardessono); 2) APN 36-090-02 (Catholic Church); 3) APN 36-330-01 (Filippi); and 4) APN 36-330-01 (Rabe). Although the development has not commenced on Altamura property, a pedestrian and bike path already has been provided along the east side of the Hopper Creek setback area. Discussion regarding the path for the Catholic Church and Filippi properties is combined with the issue of proposed new open space in Section B.1 of this chapter.

B . 3 . a

D e s i g n S t a n d a r d s

The following design standards apply to the Hopper Creek setback area:

- 1 A new path within APN 36-090-22 & 23 (Bardessono) shall be provided and connect to Heritage Estates park, and Finnell Road to the north;
- 2 A new path within APN 36-330-01 (Rabe) shall be provided and shall connect with a pedestrian bridge to Altamura property on the east side on the creek, and to the Catholic Church property on the west side of the creek;
- 3 Landscaping and maintenance standards are as established in Section B.6.

B . 4 .

T o w n P a r k s a n d S c h o o l P l a y f i e l d s

The General Plan discusses the existing Town parks or playfields and their contribution to the open space network throughout the town. The maintenance of the existing parks is important. The design standards outlined below cross reference portions of this document which are applicable for maintenance or minor changes. The Open Space/Parks Map, Figure I.8, identifies these parks or playfields by number.

B . 4 . a

D e s i g n S t a n d a r d s

The following design standards apply to all Town parks throughout town:

- 1 Landscaping and maintenance standards are as established in Section B.5;
- 2 Design standards for walls, fences, and landscape screening are as established in Section C.4;
- 3 In the event existing trees or significant landscaping is damaged and necessitates removal, replacement will be in a manner consistent with the previously existing trees or landscaping; and
- 4 New play structures or similar type of object shall be reviewed for aesthetic compatibility with adjacent properties. Landscape screening may be necessary to mitigate adverse visual impact when viewed from outside the park or playfield.

B . 5

Trees, Landscaping and Maintenance

Most of Yountville's landscape consists of plantings native to California and provide a casual unstructured feeling. As a result of the climate, most of the plants are drought tolerant and demand little maintenance. There are many mature healthy trees which contribute to the health and welfare of the town. In addition to the aesthetic qualities which make trees desirable, they also provide the following environmental benefits: erosion control, windbreaks, reduction of surface water runoff, filters for airborne pollutants, and wildlife habitats. It is the intent of the design standards to protect and preserve much of the native landscape and trees as possible.

B . 5 . a Design Standards for Trees and Landscaping

The following design standards apply to existing and new development throughout town:

- 1 Where specific heights are required for landscaping the following provisions apply:
 - a For a prescribed height of five feet or more, the initial planting shall be within two feet of the prescribed height;
 - b For a prescribed height of less than five feet, the initial planting shall be within one foot of the prescribed height. Where dense landscaping to a specified height is prescribed, the landscaping shall be of a type which will provide year-round foliage to the prescribed height, and shall be spaced such that vision of objects on the opposite side is effectively eliminated without restricting views from the sidewalk to the surrounding landscape;
- 2 All trees shall be of a species, degree of maturity, and spacing acceptable to the ZDRB or Town Council. All trees that are planted within or overhang a public street or right-of-way must be on the Town's Master Tree list as per Section 12.16.030 of the Yountville Municipal Code. Prohibited trees, as listed in Section 12.16.050, are not allowed to be planted within or overhanging a public street or right-of-way.
- 3 All planters and tree wells shall be adequately protected with bollards, metal tree skirts or curbs that are at least six inches high;
- 4 Existing mature, healthy trees shall be preserved, where possible. To ensure the successful retention of trees, the following requirements shall be met except when determined otherwise by a certified arborist:
 - a All grading around existing trees shall be done by hand;
 - b Cutting through woody roots shall not be permitted;
 - c All foundations shall step over major roots;
 - d No change of grade shall be permitted at the base of the tree;
- 5 Removal of any tree within the town limits shall not be permitted without obtaining a permit as provided in Section 6.4 of the Zoning Ordinance; and
- 6 In the case of an emergency (when a tree is dangerous to life or property), trees may be removed with the approval of the Town Administrator.

B . 5 . b

D e s i g n S t a n d a r d s f o r M a i n t e n a n c e

The following design standards apply to existing and new development throughout town:

- 1 All required planting shall be maintained in good growing condition. Such maintenance shall include, where appropriate, pruning, weeding, cleaning, fertilizing, and regular watering. Whenever necessary, planting shall be replaced with other plant materials to ensure continued compliance with applicable landscaping requirements. All screening shall be in sound structural condition, and whenever necessary, repaired and replaced.

C. INDIVIDUAL LOTS AND BUILDINGS

The Town Map does not designate lot lines for the subdivision of lands to be developed in the future. Developers are permitted within limits to respond to the conditions of the housing market at the time they build. The Design Document requires, however, that future subdivisions observe two principals that contribute to the public quality of the town. First, subdivisions must provide lots of varying size. Differences in lot size and building type characterize the older parts of town and are absent from some homogenous recent development. The design standards below are intended to ensure that new subdivisions will not be large tracts of identical house types. Second, it is important to the quality of the town that buildings collectively shape the public space of streets and open space. An over abundance of odd parcels and building orientations can result in spatial fragmentation which erodes the town's coherent sense of community. It is, therefore, an important principle that the grid of streets be reinforced by orthogonal relationships of lots and streets. Odd-shaped parcels and non-orthogonal building orientations should be avoided and occur only where needed to adapt to topographic conditions or remnant parcels left by previous development.

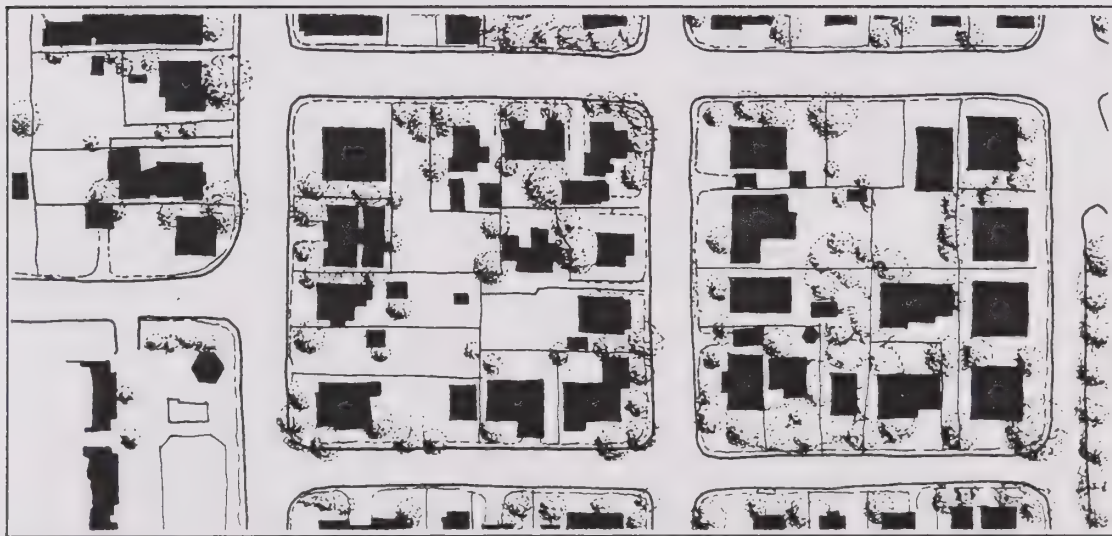


Figure I.13

Existing Pattern of Old Town

C.1. Configuration and Aggregation of Lots

In Yountville, parcels and individual lots are generally rectilinear in shape and orthogonal to the street. In Old Town, lot sizes vary but are characteristically smaller than ones in newer developments and many individual small lots make up each block. The sum of the many lots result in a fine grain scale consisting of small gardens and front yards, cottages and simple houses. The size of lots is in proportion to the buildings which occupy them. This document establishes lot sizes related to building types and uses in Chapter II, Sections C and D. The design standards outlined below intend to establish a similar rhythm and scale of lots for new development.

C.1.a

Design Standards for Lots

The following design standards apply to new development throughout town:

- 1 There shall be a variety of lot sizes with at least a 500 square foot difference between sizes. Developments with 5 to 9 lots shall have a minimum of 3 lot sizes. In developments with 10 or more lots, no one lot size shall comprise more than 25% of the total number of lots. Lots shall be predominately rectilinear in shape and orthogonal to the street. Odd-shaped parcels should be avoided and new flag lots shall be discouraged within new development. New flag lots may be permitted only when the objectives of the Town Council or General Plan are met, especially those for affordable housing;
- 2 An existing lot may be subdivided provided that the size of the newly created lots meet the minimum size established for the building location and type in Chapter II; and
- 3 Aggregation of lots is discouraged, however, lots may be aggregated provided that the size of the newly created lot does not exceed the maximum size lot established for the proposed building type in Chapter II.

C.2.

Placement of Buildings and Parking

The placement of buildings on individual lots gives a rhythm and scale to the street and neighborhood. Some of Yountville's neighborhoods have a pattern of small front and rear yards, houses with porches lining the street, and garages located in the back of the lot. The drawings below illustrate possible placement of buildings and accommodation of parking which minimizes the impact of parking while providing a visual link between private gardens and the public realm of the street.

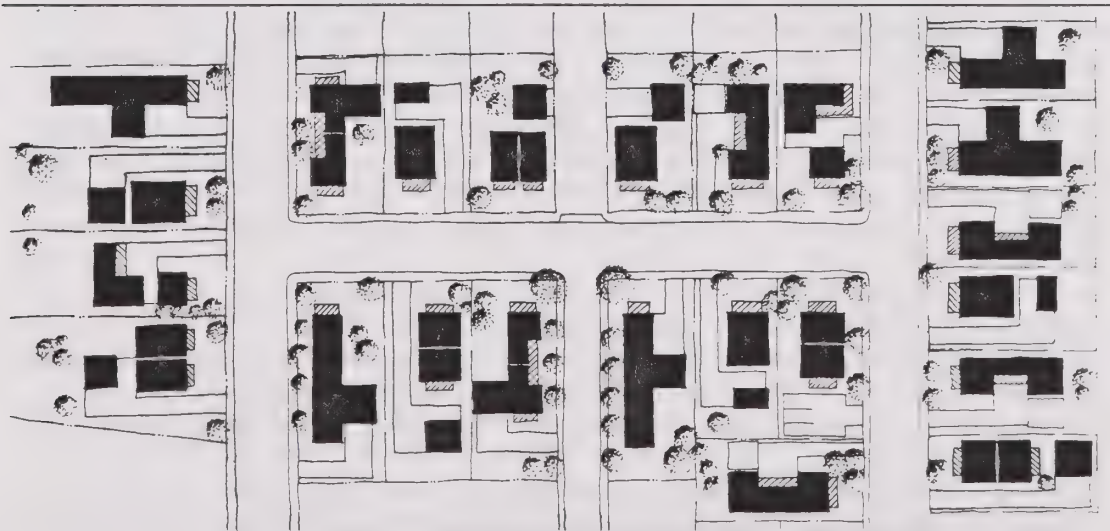


Figure I.14

Potential Pattern of New Residential Development

C.2.a Design Standards for Placement of Buildings & Parking

The following design standards apply to new development throughout town:

- 1 Architectural building elements are encouraged and may encroach into front yards as established in Section C.3.b of this chapter.
- 2 Garages are encouraged to be located in the rear half of the lot, and where possible, be accessed by an alley or shared drive. Adjacent properties are encouraged to share driveways or access drives.
3. The distance between new buildings facing each other on new residential streets and alleys shall be at least 30 feet.

C.3. Yards, Setbacks, and Encroachments

Yards provide a setback from the street and separations from adjacent buildings to ensure privacy and adequate light and air. However, it is often the cohesiveness of front and rear yards that give a special character and feeling to a street or neighborhood. Front yards extend the public space of the street into the private realm of the individual lot. Architectural elements which give a distinctive scale and texture to the yard or street often encroach into setback. Encroachments include inhabitable and uninhabitable areas, such as balconies, decks, open porches, exterior stairs, eaves, awnings, sills and chimneys. The design standards outlined below are intended to ensure that required yards remain open and unobstructed while allowing encroachments, and that new development does not deprive existing buildings of adequate light and air.

C.3.a Design Standards for Yards and Setbacks

The following design standards apply to new development throughout town:

- 1 For residential properties, the required front yard or setback shall be a minimum depth of 20 feet; and for commercial properties, at least 15 feet in depth;
- 2 For residential properties, the required side yard or setback shall be a minimum depth of five feet, except on the street side of a corner lot where the side yard or setback shall not be less than 10 feet;
- 3 For residential properties, the required rear yard or setback shall be a minimum depth of 20 feet;
- 4 For commercial properties, rear and side yards or setbacks shall be a condition of approval at the time of Master Development Plan review;
- 5 All required yards or setbacks from street frontage shall be measured from the established building line to the established public right-of-way or property line;
- 6 No accessory building shall be located in a front yard, or in a side yard on the street side of a corner lot;
- 7 Where new construction faces the major windows of an existing building on an adjacent lot, the separation between buildings shall be at least 15 feet;
- 8 Required light and air separations as defined in the Uniform Building Code, UBC, are intended to provide clear space for existing windows, and therefore, no encroachments of any kind are permitted;
- 9 Yards or other open space provided for a lot or building in order to comply with regulations of this document shall not be considered as providing a yard or open space for any other lot, building or structure, unless proposed and approved as a part of a Master Development Plan in accordance with the provisions and procedures established in Section 7.4 of the Zoning Ordinance;

- 10 For all new buildings which are part of a Master Development Plan, zero lot line may be considered for rear and side yards or setbacks for some portion of a shared property line;

C.3.b

Design Standards for Encroachments

The following design standards apply to new development throughout town:

- 1 Architectural elements up to one-story in height or 12 feet to the plate height, and used as open space, such as balconies, decks or unenclosed porches may encroach into front yards or setbacks up to eight feet in depth; but shall not be closer than 10 feet to the front property line;
- 2 Architectural elements used for building access, such as exterior stairs, roofed but open corridors, fire escapes or exterior accessways may encroach into front or rear yards or setbacks up to four feet in depth;
- 3 Architectural features, such as eaves, awnings, sills, cornices, flumes or chimneys, may encroach into front, rear or side yards or setbacks in a manner consistent with the building design but in no case greater than two feet in depth;
- 4 Bays no greater than three feet deep and 10 feet wide and no higher than two stories may project into the front setback. The maximum frequency of such bays is one bay per 15 feet of street frontage;
- 5 No habitable encroachments are permitted into required side yards or setbacks;
- 6 Mechanical equipment such as water heaters or spa pumps are not permitted to encroach into the front or side yards or setbacks. Placement within the rear yard shall maintain a minimum setback dimension of five feet. Location of air conditioners shall consider the relationship to adjacent buildings, especially bedrooms and main living areas;
- 7 A garage or other accessory structures may encroach into rear and side yards or setbacks, but must maintain a minimum setback dimension of five feet except as outlined in Chapter II, Section E.1.a; and
- 8 A garage with Second Residential Unit above may not encroach into the rear yard or setback.
- 9 Decks, whether attached or detached, may encroach into rear and side yards or setbacks, but must maintain a minimum setback dimension of 5 feet.

C.4.

Walls, Fences and Landscape Screening

Walls, fences and landscape screening often help to define the character of the street and neighborhood. They serve to provide enclosure, definition and privacy to the front yard, rear garden, or parking area. They also are intended to protect individual properties from traffic, noise, heat, glare, and dust. Therefore, walls and fences which are visible from the street should be treated as an integral part of the architecture. The materials, colors, and detailing should be consistent with the buildings they surround and adjoin. Walls, fences and landscape screening may be located along all property lines provided the conditions outlined below are met.

C . 4 . a

Design Standards

The following design standards apply to all new walls, fences or screening for existing and new development throughout town submitted for approval following the adoption of this document:

- 1 All heights are measured from finished grade, unless otherwise determined by the Town Council;
- 2 Walls, fences or landscape screening are required in the following cases:
 - a Along the rear property lines of any non-residential use which abuts on a residential use;
 - b To screen any open area used for the storage of goods, materials, or wastes from view from abutting properties and from public rights-of-way and all openings for access ways shall be constructed of view-obscuring materials;
 - c To screen from abutting properties any open area used to display goods or materials for sale;
- 3 The height of a wall or fence along a rear or side property line shall not exceed six feet;
- 4 The height of a wall or fence along a side property line shall not exceed three feet within the front yard or setback and shall not exceed six feet for the remainder of the parcel;
- 5 The height of a wall, fence or landscape screening along the front property line shall not exceed three feet and the height of a wall or fence within the front yard or setback shall not exceed three feet;
- 6 The height of a wall, fence or landscape screening a parking area or driveway shall be at least three feet but shall not exceed six feet;
- 7 Required walls, fences, or landscape screening need not be provided along a shared lot line where a wall, fence or landscape screening of equivalent height and density exists on the other side of the shared property line, and is properly maintained;
- 8 On corner lots, the height of any wall, fence or landscape screening within 35 feet of the corner of the lot at the street intersection shall not exceed three feet in height and may be required to be set back from the property line when the Planning Officer deems it necessary for public safety;
- 9 Walls, fences or landscape screening of at least three feet in height may be required by the ZDRB or Town Council to obscure from view along public rights-of-way all or part of an open area used to display goods or materials for sale, such as a lumberyard or nursery.

C . 5 .

Utilities and Refuse Storage

Provisions for utilities including accessory equipment, and storage for refuse are often overlooked in the planning and site design of a new development. Their misplacement results in unreconciled intrusions into yards and streetscapes. The design standards outlined below are intended to ensure that yards and streetscapes remain free from ill-considered utilities or refuse storage and adequate accommodation is provided.

C.5.a

Design Standards

The following design standards apply to utilities and refuse storage for new development throughout town:

- 1 Utilities and refuse storage areas are not permitted in any setback area or front yard except as noted in #2 below;
- 2 All new electrical, telephone, CATV and similar service wires or cables shall be installed underground. Risers on poles or buildings are permitted. Where there is no underground system, utility service poles may be placed on the rear property line solely for the purpose of terminating underground facilities;
- 3 Air conditioners and similar mechanical equipment shall be screened from view;
- 4 Electrical vaults and meter boxes must be screened from view and discreetly located. Fire pipes and extinguishers must be easily identified, but discreetly locate; and
- 5 For Commercial Uses: Refuse storage areas shall be screened from public and adjacent properties view or located within a building. All refuse storage areas shall be maintained to the reasonable satisfaction of the Town Council.

D.

OLD TOWN HISTORIC DISTRICT DESIGN

It is the intention of this section of the Design Ordinance to provide conceptual guidance as well as specific building guidelines to those wishing to build or remodel within the H District boundaries.

The Old Town area is just that; it is where the first residences were located in Yountville. The houses are, for the most part, individually designed, and built with attention to trim and other detail. Every effort shall be made to save these existing structures. Their individuality is one of the things that gives the neighborhood the unique character that has been determined worthy of protection and preservation.

Old Town has a very soft, rural look. There are no curbs, gutters, or sidewalks. The hard edges of the roads are softened with gravel or dirt in the rights-of-way, and the houses are further sheltered from the street by mature plantings, low wooden fences, or both. There are generally no unrelieved expanses of lawn. The emphasis is on informal small gardens. The buildings themselves are constructed mostly of natural materials, predominately wood.

The human scale and orientation of Old Town is an important factor when considering new construction. The overall look of the buildings is low and small; although there are some two-story residences scattered through the area, these larger homes are few in number; therefore, they add interest, break up roof heights, and are often on the larger parcels. Most homes in the area were built at a time when the living space and a friendly front porch were dominant in the design of the front elevation. Unlike the modern suburban ranch-style house, the garage is secondary or hidden behind the living space.

Variation, detail, softness, and an attention to human need and aesthetics are all important issues in Old Town construction applications.

D.1

Height and Setbacks

Building height shall be measured by external appearance and shall include lofts, dormers, and attics. Varied roof heights and front setbacks are encouraged to give individuality to each structure, especially when there are two or more adjacent lots to

be developed. To promote the variation in roof heights all setbacks are proportional to the height of a building or structure.

D.1.a

Design Standards
for Setbacks of Primary Buildings

- 1 For all new developments and additions in the Old Town Historic District the setbacks from property lines shall be as follows:
 - a Front: for buildings or portions of building up to 15 feet high, at least a 10 foot setback;
 - b Front: for buildings or portions of buildings 15 - 18 feet high, at least a 15 foot setback;
 - c Front: for buildings or portions of buildings over 18 feet high, at least a 20 feet setback;
 - d Front: for garages, at least 18 feet;
 - e Rear: for the first story, at least 15 feet; and for the second story, at least 20 feet;
 - f Side: for one story buildings, at least 5 feet, and a total of at least 13 feet for both sides. If the lot is less than 50 feet wide, then both side setbacks must be a minimum of 5 feet for a total of 10 feet.
 - g Side: for two-story buildings, 8 feet minimum for the second level, and total of at least 22 feet for both sides. If the lot is less than 50 feet wide, then 8 foot minimum for the second level, for a total of 16 feet.
 - h Side: on a corner lot, 10 feet minimum for the first level, and at least 16 feet for second level.

D.1.b

Design Standards for Accessory Buildings

- 1 For all new accessory buildings, the setbacks from property lines shall be as follows, except as outlined in Chapter II, Section E.1.a:
 - a Side: for buildings up to 15 feet high, at least 5 feet;
 - b Side: for second story over a garage, at least 8 feet; and
 - c Rear: for second story over a garage, at least 20 feet.

D.2

Roofs

The main roof of the building is encouraged to have a minimum pitch of 4 in 12. Roofs with multiple ridges, eaves, dormers, and more steeply varied pitches are encouraged. A minor portion of a structure's roof may be flat where architecturally indicated in order to achieve a design style. Roofing should be wood, composition shingle, clay tile, or similar style material. Any other materials shall be submitted to the Zoning/Design Review Board. The use of common concrete tile, tar and gravel, multi-colored asphalt, or common metal is prohibited on the main roof.

D.3

Garages

It is a goal of H District planning to make permanent parking places and garages less visible and dominant in the overall design. To accomplish this, detached garages and garage doors that do not face the street are encouraged. Single car garages may be located near the front of the lot, though this is not encouraged. Garages in the front half of the lot shall have a maximum door width of 12 feet. Double car garages are permitted if not visible from the street or if located on the rear half of the lot.

D.4

Exterior Colors

Exterior color samples for new construction shall be submitted to the Zoning/Design Review Board. Designers are encouraged to use muted and soft colors compatible with Old Town.

D.5

Windows

The windows, as well as doors, are an important architectural detail. Multi-paned windows and doors are encouraged if visible from the street. Picture windows and sliding glass doors or windows are discouraged where visible from the street. The look of double-hung windows is more in keeping with the area. Only wood-frame windows, vinyl-clad wood windows, or metal framed windows colored by the manufacturer to complement the exterior finish are permitted. Bare metal, plastic, or silver-colored aluminum windows or screen frames shall not be approved for design review.

D.6

Porches

A covered porch should be a dominant design feature of the front elevation. The porch sets the tone for the house as well as the neighborhood. Large porches are strongly encouraged. The porch should be at least 72 square feet and shall measure at least 6 feet deep (measured perpendicular to the front wall of the house). Porches may be screened, but may not be enclosed with more substantial material. They are intended to be open and airy and not to be incorporated formally into the house. No porch shall encroach within 10 feet of the front property line.

D.7

Gardens and Landscaping

Street trees shall be planted per Town requirements. Impermeable, inorganic, or lifeless ground covers such as concrete or ornamental rock are discouraged as landscaping in front yards. (This does not exclude their use for walks and pathways) Informal landscaping which emphasizes mature, drought tolerant or native plant varieties is encouraged. Low picket fences at the front of property lines are encouraged. Chain-link fences are prohibited in front yards. Utilities and trash dumpsters shall be screened from the street. Landscape plans shall be reviewed at time of design review.

D.8

Driveways

Driveways and transition aprons are encouraged to be surfaced with permeable materials such as gravel or decomposed granite. Asphalt and concrete are discouraged except where consistent with existing sidewalk conditions.

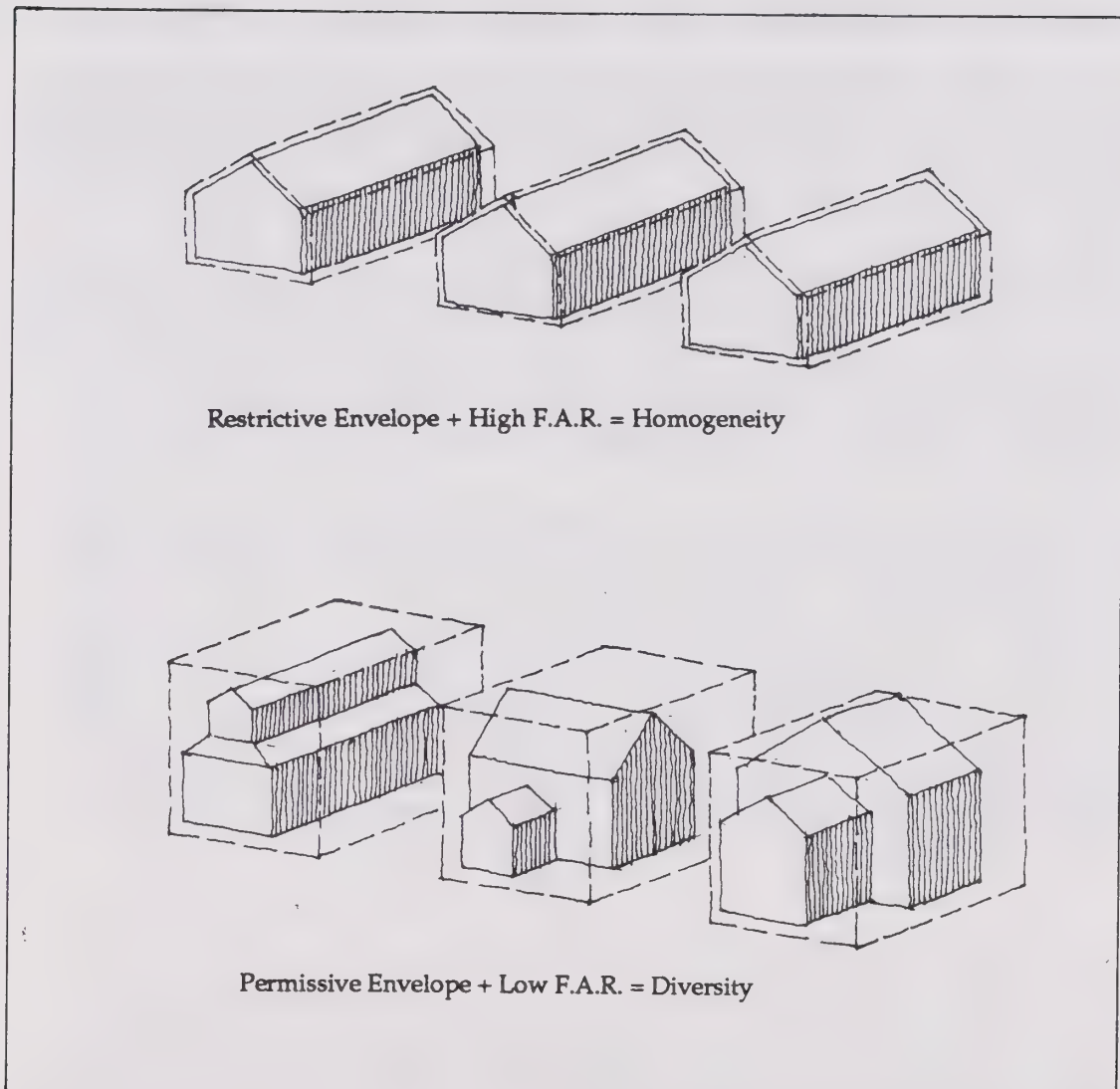


Figure II.1

Sketches of Building Envelope versus FAR

CHAPTER II. BUILDING TYPES & ARCHITECTURAL STANDARDS

The General Plan emphasizes creating a Town Structure according to gridiron planning principles. For Yountville, it is necessary that the Town Structure be reinforced by the quality and organization of individual buildings. Some types of buildings help to perpetuate the town's rural scale, and others are damaging. The intention of this chapter is to encourage the production of locally appropriate building types, and to prohibit the continued intrusion of inappropriate ones. To the maximum extent possible, this document seeks to maintain the rhythm, scale, and diversity of buildings typical of Old Town. The design standards are directed toward ensuring a variety of building designs including street frontage, roof pitch, and architectural elements. Buildings which are designed, detailed and constructed with care and consistency are encouraged. The overall intent is to invite buildings with uses which reinforce the walkable aspect of the town and do not demand a substantial increase in parking.

All buildings are regulated by Floor Area Ratio, FAR, which establishes a maximum permitted square footage, and height restrictions. It is intended to encourage a variety of building massing and of architectural style while disallowing the full build-out of the building envelope. The drawing to the left illustrates the difference between regulating building massing by floor area ratio or by conventional envelope restrictions. One often results in homogeneity, while the other encourages diversity. The permitted FAR differs according to building type, and whether the lot is within an existing development, part of Old Town or part of a new development. Building heights are measured in two ways, they are measured from the average finished grade around the building to the plate height; there is also a maximum permitted height for the overall building, including a pitched roof or other architectural feature.

A .

THE CRAFT OF BUILDING

Yountville's architectural heritage is not stylistically homogenous, and this document is not prescriptive about architectural style. Most of the existing buildings are simple shapes and of wood construction. The details of buildings are consistent with the selected materials. The town and its buildings benefit from incorporating architectural features that are constructed with special care and skill. Therefore, there are a few principles of design and craft which all new buildings in Yountville should follow.

A . 1 .

B u i l d i n g M a t e r i a l s

New buildings should support regional traditions and maintain a level of craft in the process of construction. The choice, detailing and application of materials should be carefully considered for their appropriateness with the design of the new buildings.

A . 1 . a

D e s i g n S t a n d a r d s f o r B u i l d i n g M a t e r i a l s

Listed below are materials or combinations of materials which are permitted and should be encouraged; and others which should be avoided, and are not permitted. The following design standards apply to new construction throughout town:

- 1 Buildings shall have consistent materials and details throughout. Materials and details shall be of a similar architectural theme on all sides of buildings. Materials which appear veneer-like should be avoided, and seams, joints or raw edges of materials shall be concealed;
- 2 Exterior finish materials shall appear integral to the building construction. Material or color changes at the outside corner of buildings give an impression of thinness and artificiality and should be avoided. Changes of exterior color, texture or material shall be accompanied by changes in plane, with exceptions for the base of buildings, corner boards, gable ends or similar conditions;
- 3 Exterior finish materials shall be of solid wood, natural stone, brick, or stucco; window casings and building trim shall be solid wood; except as follows:
 - a Use of manufactured materials that simulate the above materials may be permitted if an applicant can show, to the satisfaction of the ZDRB or Town Council, that the proposed material successfully simulate the above stated material. Samples of the proposed material or existing examples of its use, shall be provided at the time of Design Review;
 - b Other material may be used if the applicant can show, to the satisfaction of the ZDRB or Town Council, that the proposed material is appropriate and creates a building or structure that is architecturally and aesthetically compatible with its surrounding area;
- 4 Window and door style shall be compatible with overall building design. Flush nail-on windows shall not be used in combination with rough textured stucco. Spanish colonial style buildings shall not have window frames flush with the outside plane of a wall; and
- 5 Exceptions to the strict adherence to the above 1-4 may be granted by the ZDRB provided that the design does not compromise the overall intent and purpose of this Section.

B .

R E L A T I O N O F B U I L D I N G S T O S T R E E T S

One of the principal objectives of this document is to ensure that the streets of Yountville remain animated by dwellings, businesses or other activities, and do not become lifeless blocks of blank walls, parking lots, driveways or garage doors. Most residential buildings have architectural elements which serve many purposes, and give a domestic scale to the street frontage. One example is the front porch which usually serves as an entrance, and also as an outdoor living/open space. Commercial buildings, however, address the street in various ways. Some commercial buildings reinforce the street frontage with residential-scaled building elements, while other commercial properties have street frontage dominated by large areas of paving. The design standards outlined below intend to establish street frontages which are defined by architectural elements of buildings where people live, work, shop and relax.

B . 1 . B u i l d i n g F r o n t a g e a n d O r i e n t a t i o n

The embellishment of entrances animates streets, gives identity to individual dwelling units or commercial uses, and helps give a human scale to the town. Architectural building elements in the form of unenclosed porches, or recesses in buildings may serve as entrances for several dwelling units or businesses, and make a cohesive street frontage.

B . 1 . a D e s i g n S t a n d a r d s

The following design standards apply to new development throughout town:

- 1 The principal orientation of all buildings shall be parallel to streets they face. Accessory buildings may be exempt;
- 2 The street elevation of multiple unit residential buildings shall have at least one street-oriented entrance, and contain the principal windows of the front unit; and
- 3 For single-family houses with attached garages, the width of the house shall be at least five feet greater than the width of the garage along its street frontage. The garage shall be set back at least 10 feet farther than the house from the street.

B . 2 . B u i l d i n g E l e m e n t s

Architectural building elements such as unenclosed porches, balconies, loggias, bay windows and entrances are an essential part of the character that make streets and neighborhoods feel walkable. To encourage the use of this kind of building elements 250 square feet of area exemption are allowed from FAR calculations as outlined later in this chapter. The intent is to strengthen and enhance the architectural character while not impeding accessibility.

B . 2 . a D e s i g n S t a n d a r d s

The following design standards apply to new development throughout town, with the exception of Primary Commercial:

- 1 The street frontage of new buildings shall contain one of the following elements:
 - a Upper floor loggias recessed within the building;
 - b Roofed balconies supported by brackets or by columns at the ground level;
 - c Single-story or two-story unenclosed porches;
 - d Exterior wooden or masonry stairs with closed risers; and
 - e Building elements similar to those above and deemed acceptable by the Town Council or ZDRB.

C . R E S I D E N T I A L B U I L D I N G S

Single-family houses and multi-family housing take on many shapes and sizes, and often only address issues of a particular site or desires of its inhabitants. Therefore, it is the intent of this document to establish design standards which ensure each single-family house, duplex or apartment house contribute to the making of a residential street and neighborhood. The hope is that no two houses be the same but that a thread of common architectural elements give a cohesive scale and character to streets, and help form neighborhoods. Design standards are established for each building type and include single-family houses, duplexes, triplexes, fourplexes, apartments, second units, and manufactured housing.

C.1.

Single-family Houses

In Yountville, there is a strong heritage of diverse single-family houses. Some are one-story, some are two-story; some are large, but many are quite small; some are situated in a vineyard or orchard, and others are five feet from the street. However, there is a cohesive scale and character that is rural. In addition to yards, other open space such as terraces, courtyards and small gardens extend the living space to the outdoors. The design standards outlined below intend to maintain and enhance the existing mix of single-family houses through the development of individual lots within the few remaining parcels with similar types of houses.

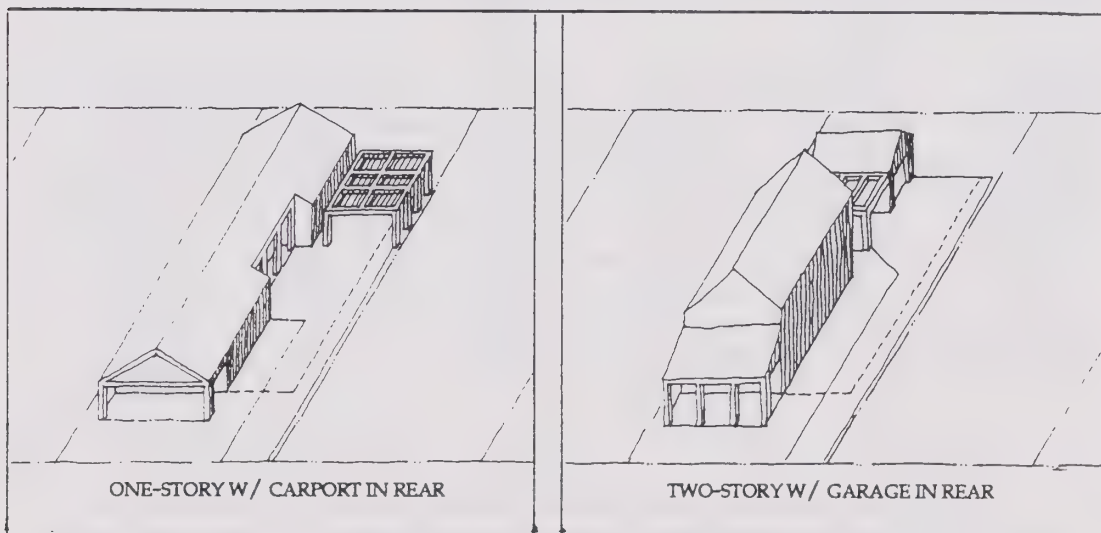


Figure II.2a

Examples of Single-family Houses

C.1.a

Design Standards

The following standards apply to new single-family houses within single-family and mixed residential land-use designations, other new development, and improvements of existing single family houses throughout town:

- 1 **Lot Area:** The minimum lot area shall be 4,000 square feet, and the maximum lot area shall be 8,000 square feet. In Old Town, the minimum lot area shall remain at the current limit of 5,000 square feet. Where a FAR Density Bonus is granted, no more than 25 percent of the total number of lots shall be 6,500 square feet or larger;
- 2 **Floor Area Ratio:** The base level Floor Area Ratio throughout Town for new single family homes on individual lots shall be 0.25.
 - a. Exceptions: The RS District and H District areas of Town have been examined to determine existing patterns of single family residential development. Based on the character within these areas, the FAR for existing homes in these areas is allowed to exceed 0.25 FAR in order to allow modest additions that are consistent with the housing pattern of the neighborhood. Exceptions to the 0.25 FAR are allowed for existing and new homes in the Single Family Residential (RS) district and Old Town (H) district as outlined below.
 - 1) **RS District:** A maximum 0.30 FAR is allowed for the living area of the home and a maximum 0.35 FAR is allowed for the gross building square footage including garage on lots up to 8,000 square feet. A maximum of 0.25 FAR living area and a maximum of 0.30 FAR for gross area is allowed on lots over 8,000

*Design Ordinance
Building Types and Architectural Standards*

square feet. There are no exemptions for garages or carports. Exemption for other items are allowed as noted below.

- 2) Historic District Small Lots: New homes and expansion of existing homes on small lots (defined as any lot under 5,000 square feet) are allowed a 0.25 FAR plus a 200 square foot garage exemption for a garage or carport on the lot. For lots 4,000 square feet or less a base living area of 1,000 square feet is allowed plus a 200 square foot garage/carport exemption.

LOT SIZE - FAR MATRIX
WITH MINIMUM LIVING AREA

Lot Size	Base Living Area	Area @ 0.25 FAR	Garage Exemption	Total Square Feet	Floor Area Ratio
3,000	1,000		200	1,200	0.40
3,500	1,000		200	1,200	0.34
4,000	1,000		200	1,200	0.30
4,100		1,025	200	1,225	0.298
4,200		1,050	200	1,250	0.297
4,300		1,075	200	1,275	0.296
4,400		1,100	200	1,300	0.295
4,500		1,125	200	1,325	0.294
4,600		1,150	200	1,350	0.293
4,700		1,175	200	1,375	0.292
4,800		1,200	200	1,400	0.291
4,900		1,225	200	1,425	0.290

b Exemptions:

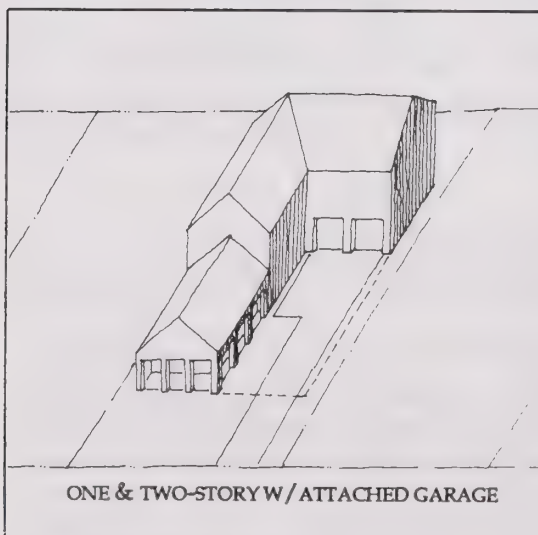
- 1) Garages and Carports: An exemption of up to 400 square feet is allowed for garages and carports located entirely within the rear half of the lot. (Does not apply in RS District or in the H District for lots under 5,000 square feet)
 - 2) Second Residential Units: An exemption of up to 300 square feet is allowed for a legally permitted Second Residential Unit.
 - 3) Architectural Elements: A combined Exemption of up to 1,000 square feet is allowed for all the architectural elements of a home (see definition in Zoning Ordinance). The calculation of the exemption for unenclosed porches shall be based on the ratio of 4 square feet of porch area for each lineal foot of lot street frontage up to the maximum allowed. The minimum porch area exemption, regardless of lot street frontage, shall be 250 square foot. (Revised 10/11/94 Ordinance Number 252-94)
 - 4) Accessory Structures: A combined Exemption of up to 100 square feet is allowed for all enclosed accessory structures on a lot located in the rear yard and screened from street view , excluding carports, garages, and second residential units.
 - 5) Landscape Elements: Landscape elements including detached decks under 30 inches in height are exempt from FAR calculations.
- 3) Affordable Housing FAR Density Bonus: An FAR density bonus is available for projects of 5 or more lots which have been approved as part of a Master Development Plan and have 10% or more onsite affordable housing units. The FAR density bonus is allowed on a graduated scale according to lot size and the percentage of onsite affordable housing.

Design Ordinance
Building Types and Architectural Standards

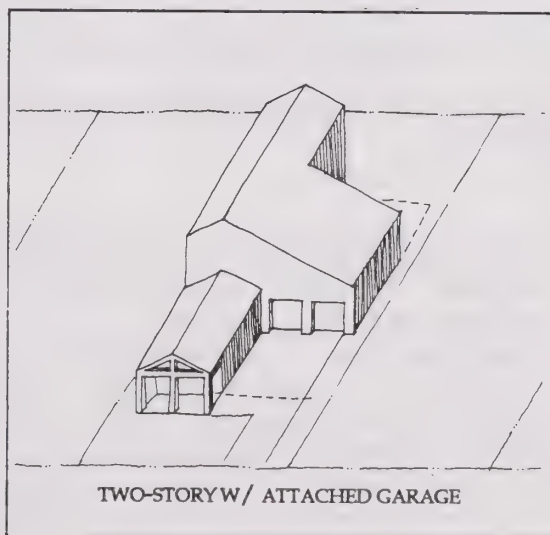
	Lot Size	Affordable Percentage		
		10%	15%	20%
a)	4,000 - 4,999 square feet	.25	.275	.30
b)	5,000 - 5,999 square feet	.30	.325	.35
c)	6,000 - 8,000 square feet	.30	.35	.40

There is no FAR bonus for homes on lots over 8,000 square feet.

- 4 Master Planned Residential District: The FAR's in the MPR District shall conform to the lot specific FAR's and building footprints in the adopted Master Plan. If there is no specified FAR or building foot print for a lot, the FAR shall be the base level 0.25.
- 5 Building Height: The building height for a single-family house shall not exceed two stories. One-story houses shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story houses shall not exceed 20 feet to the plate height or an overall height of 28 feet. No more than 50 percent of the single-family dwelling units in any given block are permitted to be two-story. Where this percentage is currently exceeded, no new two-story structures will be allowed. A block shall mean both sides of the street lying between intersecting streets. Corner houses shall be counted on both street elevations. The ZDRB or the Town Council may grant an exception to the 50 percent limitation subject to criteria approved by resolution of the Town Council.



ONE & TWO-STORY W/ ATTACHED GARAGE



TWO-STORY W/ ATTACHED GARAGE

Figure II.2b

Examples of Single-family Houses

For height limits pertaining to accessory buildings, and second residential units refer to Section E, and C.5, respectively ; and

- 6 Open Space: There are no minimum area requirements for open space.

C.2.

Duplexes

Duplexes have a significant history in American cities and towns, however, in Yountville there are only a few. Although there are always two separate dwelling units, duplexes usually appear as a large single-family house from the street. Some are paired townhouses, and in other cases, it is a two-unit building in which the building owner lives in one and rents the other. The General Plan seeks to encourage this type of small scale multi-family housing on vacant parcels within existing neighborhoods, and for some portion of new developments. It is important that each dwelling unit have its own entrance and private open space, however, a duplex building should appear as a coherent whole from the street. The design standards apply to all duplexes, including side-by-side dwelling units, back-to-back dwelling units, and stacked dwelling units.

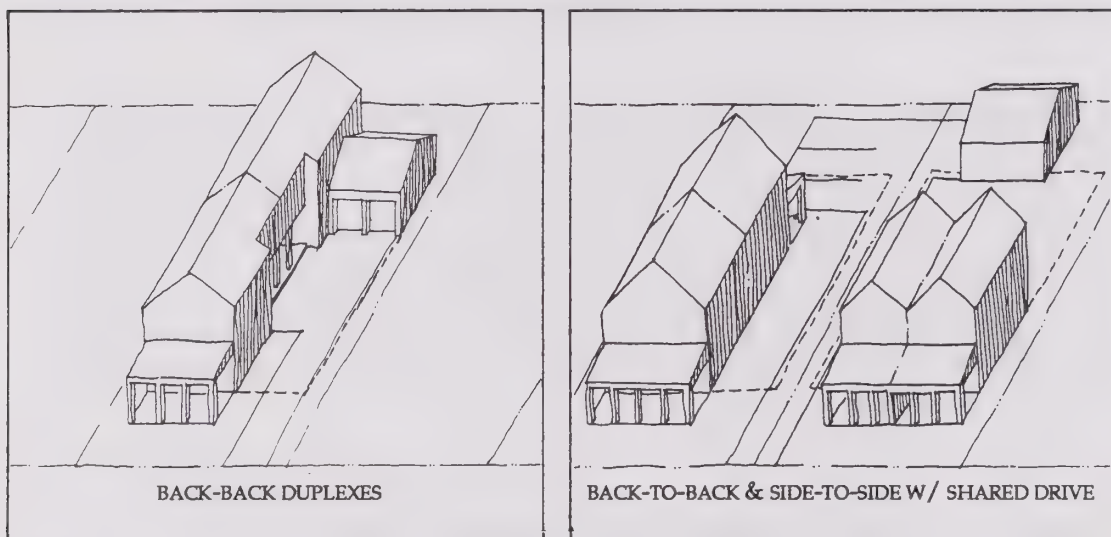


Figure II.3

Examples of Duplexes

C.2.a

Design Standards

The following standards apply to duplexes in new development throughout town:

- 1 **Lot Area:** The minimum lot area shall be 5,000 square feet, and the maximum lot area shall be 8,000 square feet;
- 2 **Floor Area Ratio:** The permitted Floor Area Ratio shall be 0.4 FAR on all lots. Exemptions include up to 250 square feet of area occupied by unenclosed porches, balconies, breezeways, and similar architectural elements. Garages and carports which are located within the rear half of the lot are exempt up to 400 square feet;
- 3 **Building Height:** The building height for a duplex shall not exceed two stories. One-story duplexes shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story duplexes shall not exceed 20 feet to the plate height or an overall height of 28 feet. For accessory buildings refer to Section E;
- 4 **Open Space:** The minimum area required for use as private open space by each dwelling unit shall be 200 square feet and shall be directly accessible to its unit. For second story units, an exception may allow half of the private open space to be directly accessible to the unit with the remaining portion provided at grade; and
- 5 **Parking:** Surface parking may encroach up to 15 feet into the rear yard or setback but not into open space, provided that a wall, fence or landscape adequately screens the view of parked cars from the street or adjacent properties, see Chapter I, Section C.4.

C.3. Triplexes, Fourplexes and Apartments

There are a few triplexes, fourplexes or apartment buildings currently within the town. While these types of residential buildings provide an alternative to individual home ownership, they also can contribute to the making of neighborhoods comprised of diverse yet compatible buildings. Residential buildings of these types are often clustered together around a courtyard or common garden, and have a massing and scale similar to single-family houses. The design standards outlined below are intended to ensure the compatibility of multi-family residential buildings with single-family houses in ways which encourage new development to provide a mix of residential building types similar to Old Town.

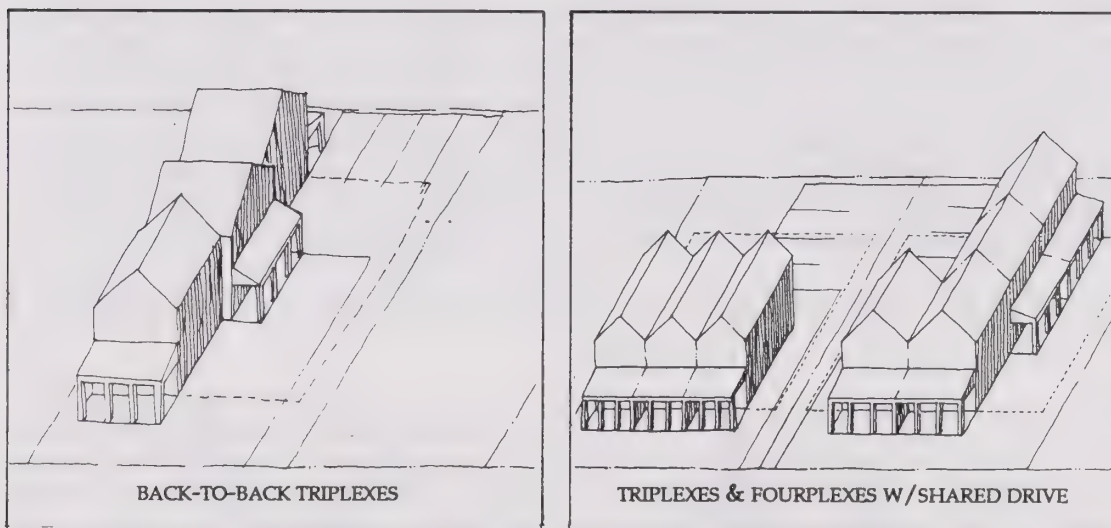


Figure II.4

Examples of Triplexes and Fourplexes Buildings

C.3.a

Design Standards

The following standards shall apply to buildings within the Historic District, Mixed Residential, Primary Commercial and Residential-scaled Commercial land-use designations:

- 1 Lot Area: The minimum lot area shall be 6,000 square feet, and the maximum 8,000 square feet;
- 2 Floor Area Ratio: The permitted Floor Area Ratio shall be 0.40 FAR on all lots. Infill lots not assigned a density bonus range for affordable housing in Table III.20 of the General Plan are allowed both a unit and FAR density bonus when 20% or more affordable housing is provided on site as follows:

	Unit Bonus	FAR
20% or more affordable	40%	.56

Exemptions include up to 100 square feet per unit of area occupied by unenclosed porches, balconies, breezeways, and similar architectural elements. Garages and carports which are located within the rear half of the lot are exempt up to 200 square feet per unit;

- 3 Building Height: The building height for multi-unit buildings shall not exceed two stories. One-story multi-unit buildings shall not exceed 15 feet to the plate height or

an overall height of 20 feet. Two-story multi-unit buildings shall not exceed 20 feet to the plate height or an overall height of 28 feet. Accessory buildings see Section E;

- 4 Open Space: The minimum area for use as private open space by each dwelling unit shall be 100 square feet and shall be directly accessible to its unit. For second story units, an exception may allow half of the private open space to be directly accessible to the unit with the remaining portion provided at grade; and
- 5 Parking: Surface parking may encroach up to 15 feet into the rear yard or setback but not into open space, provided that a wall, fence or landscape adequately screens the view of parked cars from the street or adjacent properties, see Chapter I, Section C.4.

C.4.

Second Residential Units

Second residential units on single-family lots occur throughout Yountville, and can serve an increasing demand for rental housing. There are three types of buildings which generally accommodate second residential units; 1) a detached building or cottage which is located either at the front or rear of the lot, 2) an attached second unit, and 3) a studio apartment over a garage usually located at the rear of the lot. The design standards below are intended to ensure that second units provide an adequate standard of living and do not adversely impact adjacent neighbors, especially their privacy. Additional standards are contained in the Zoning Ordinance Section 5.1.

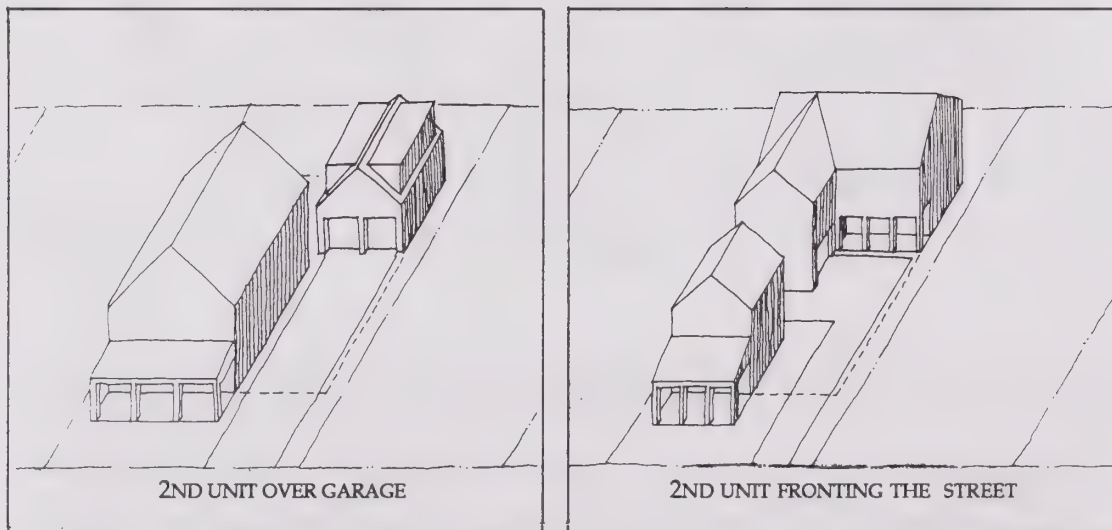


Figure II.5

Examples of Second Units

C . 4 . a

D e s i g n S t a n d a r d s

The following standards apply to Second Residential Units:

- 1 Lot Area: The lot area shall be at least 5,000 square feet when approved as part of a Master Development Plan. For lots in RS, single-family residential the lot area shall be at least 6,000 square feet , and in Old Town, at least 6,500 square feet;
- 2 Floor Area Ratio: The square footage of any second unit shall not exceed 600 square feet, and up to 300 square feet may be exempt from FAR calculations;
- 3 Building Height: The building height for second units shall not exceed two stories. One-story second units shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story second units shall not exceed 20 feet to the plate height or an overall height of 28 feet;
- 4 Open Space: The minimum area for use as common open space by a second unit shall be 100 square feet, of which half shall be directly accessible to its unit. The front and rear yards shall be considered common open space, accessible to and for use by residents of both the primary and second unit;
- 5 Parking: One parking space is required for each second residential unit, and shall be provided adjacent to the dwelling unit. Surface parking may encroach up to 15 feet into the rear setback but may not encroach into open space, and a wall, fence or landscaping shall screen the view of parked cars from the street or adjacent properties, see Chapter I, Section C.4.; and
- 6 Individual Entrance: Each second residential unit shall have its own entrance.

C . 5 .

M a n u f a c t u r e d H o u s i n g

Manufactured housing can be a viable alternative for affordable housing. Although mobile homes are the predominant type, there is an increasing variety of pre-assembled houses available. With special care and design, these types of homes can integrate into existing neighborhoods, and be compatible with adjacent houses. The design standards outlined below are intended to ensure that manufactured housing be of similar scale and character as its neighbors.

C . 5 . a

D e s i g n S t a n d a r d s

The following standards apply to new manufactured housing throughout town:

- 1 Lot Area: The minimum and maximum lot areas established for each building type shall be the same for manufactured housing of the same type;
- 2 Floor Area Ratio: The maximum floor area ratio established for each building type shall be the same for manufactured housing of the same type;
- 3 Building Height: The building height for manufactured housing shall not exceed two stories. One-story buildings shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story buildings shall not exceed 20 feet to the plate height or an overall height of 28 feet;
- 4 Open Space: The minimum area for use as private open space established for each building type shall be the same for manufactured housing of the same type; and
- 5 Foundations: The foundation of all manufactured housing shall be permanent and similar in character to the surrounding buildings.

D .

C O M M E R C I A L B U I L D I N G S

In most cases commercial buildings tend to be more versatile, and less typological in shape or form than residential buildings. In Yountville, there is a diversity of commercial buildings which accommodates many different businesses including restaurants, inns, professional offices, and many shops occupying renovated buildings that once served as a winery, a train station, or residences. However, it is more often the placement of buildings and parking on the site which has a greater impact on the character of a street or development than the buildings alone. Therefore, this section regulates new commercial buildings according to the land-use designations established in the General Plan and Zoning Ordinance, which are Primary Commercial and Residential-scaled Commercial.

The design standards for commercial uses are intended to reinforce the walkable aspect of the town and to prevent new commercial development from changing the town into a visitor-oriented commercial strip. The purpose of the residential-scaled commercial land-use designation is to encourage buildings that are pedestrian-oriented, similar in size and scale to neighboring residential buildings and accommodate a variety of commercial uses which serve the needs of the town's residents. To encourage integrated uses there is one building type, combined commercial with residential, which may be built in all commercial areas and has its own design standards. The overall intent is that all new commercial buildings are street-oriented and reinforce Washington Street as the main commercial street in Yountville.

D . 1 .

P r i m a r y C o m m e r c i a l B u i l d i n g s

Since growth of visitor-oriented businesses in residential neighborhoods is not desired, the locations for primary commercial buildings are limited to properties on the west side of Washington Street. It is intended that these buildings reinforce Washington Street as Yountville's commercial main street, while accommodating the needs of merchants. Therefore, the design standards outlined below prevent new development of auto-oriented strip commercial buildings, and ensure that new commercial uses will contribute to the rural, small town character. For these properties, it is less an issue of the building type and more an issue of the placement of buildings and site planning. New buildings should address Washington Street, and accommodate professional office buildings, small inns or other uses which provide some benefit for the town's residents.

D . 1 . a

D e s i g n S t a n d a r d s

The following design standards apply to new development throughout the town:

- 1 Floor Area Ratio: The maximum permitted Floor Area Ratio shall be .25 FAR. An additional 0.05 FAR for residential uses such as a second residential unit or an apartment is allowed. Exemption includes up to 250 square feet of unenclosed porches, balconies, attached carports, breezeways, and similar architectural elements.
- 2 Building Height: The building height shall not exceed two stories. One-story buildings shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story buildings shall not exceed 22 feet to the plate height or an overall height of 30 feet. No more than one third of the total permitted FAR for a commercial development may occupy the second level.
- 3 Open Space: A minimum of 25 percent of the gross area of a given parcel shall be provided as open space in compliance with Section 6.4 of the Zoning Ordinance. This requirement is intended to provide for open spaces, such as courtyard, which are

Design Ordinance
Building Types and Architectural Standards

unenclosed and usable to shoppers, visitors and general public and shall be proportionately scaled to allow sufficient sunlight. All such open space areas shall be in addition to walkways adjacent to Town streets. All commercial open space shall be visually accessible to the public, and should not require entry into a building for its sole access. Most open space should be physically accessible to the public.

- 4 Parking: Parking ratios related to specific uses shall be as established in Section 6.2 of the Zoning Ordinance. Landscaping and screening of parking lots shall comply with Section A.2 of this document;
- 5 Signage: All signage shall comply with Section 6.3 of the Zoning Ordinance; and
- 6 Design Review Special Requirements: Story poles or models which illustrate the 3-dimensional massing and architectural character of the proposed new buildings, including adjacent existing buildings, may be required in addition to the requirements established in Section 7.3, Design Review, of the Zoning Ordinance if in the opinion of the ZDRB such information is necessary to evaluate the impact.

D.2. Residential-scaled Commercial Buildings

Residential-scaled commercial is a building type and land-use classification intended to effect a transition between commercial and residential areas. This scale of local-serving commercial buildings and their uses add convenience, interest and livability to the town and its residential neighborhoods. Typically, these buildings are similar in size to single-family houses and other residential buildings in adjoining neighborhoods. The design standards outlined below intend to encourage the development of multiple small pedestrian-oriented buildings, and prevent the new development of an auto-oriented commercial strip within the town. To reinforce an important principle of the General Plan, the emphasis is on civic design and buildings that are compatible with the town's character. Where civic amenity is maintained or newly provided, criteria has been established for possible reduction in parking required for new development.

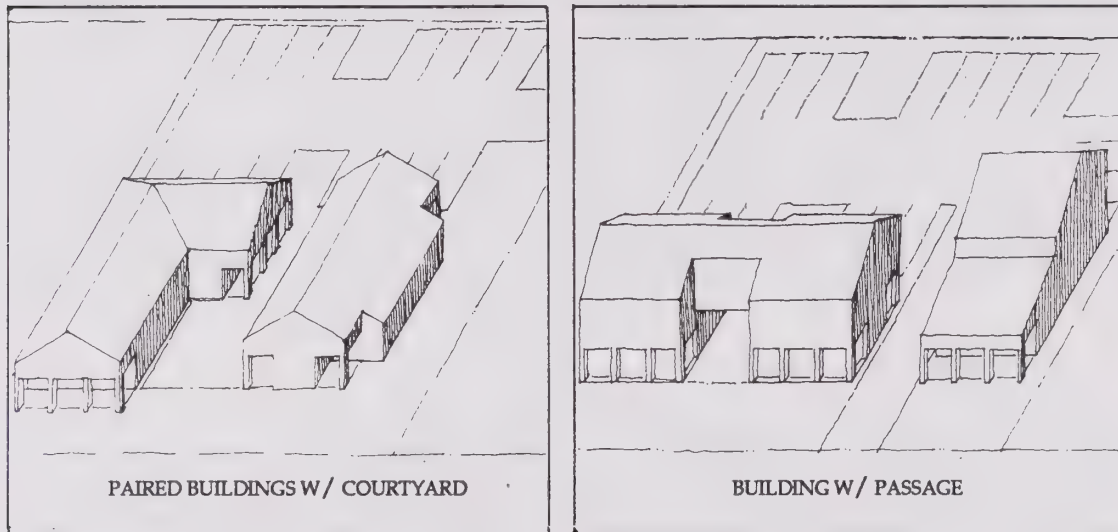


Figure II.6 Examples of Residential-scaled Commercial Buildings

D.2.a Design Standards

The following design standards apply to new development throughout the town:

- 1 **Floor Area Ratio:** The maximum permitted Floor Area Ratio shall be 0.25 FAR. An additional 0.05 FAR for residential uses such as a second residential unit or an apartment is allowed. Exemption includes up to 250 square feet of unenclosed porches, balconies, attached carports, breezeways, and similar architectural elements.
- 2 **Building Size:** Where possible proposed development should consist of multiple small buildings rather than one large building. In cases where the permitted square footage exceeds 3,200 square feet consideration shall be given to providing multiple buildings; and where the permitted square footage exceeds 5,000 square feet the proposed new development shall consist of at least two buildings. Building size and location should take into account existing trees or similar site conditions which are considered important to the town's character;

Design Ordinance
Building Types and Architectural Standards

- 3 Building Height: The building height shall not exceed two stories. One-story buildings shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story buildings shall not exceed 22 feet to the plate height or an overall height of 30 feet. No more than one third of the total permitted FAR for a commercial development may occupy the second level. The maximum height is intended to ensure an variety of roof configurations;
- 4 Open Space: A minimum of 25 percent of the gross area of a given parcel shall be provided as open space in compliance with Section 6.4 of the Zoning Ordinance. This requirement is intended to provide for open spaces, such as courtyard, which are unenclosed and usable to shoppers, visitors and general public and shall be proportionately scaled to allow sufficient sunlight. All such open space areas shall be in addition to walkways adjacent to Town streets. All open space shall be visually accessible to the public, and should not require entry into a building for its sole access. Most open space should be physically accessible to the public.
- 5 Parking: Parking ratios shall be as established in Section 6.2 of the Zoning Ordinance. Parking lots shall be adequately screened, and comply with design standards established in Section A.2 of this document. In cases where a reduced parking requirement is under consideration the following design criteria shall apply: the proposed design utilizes the area gained by the reduction in parking in ways which will help maintain the town's character without adversely impacting surrounding properties; and the proposed design incorporate the features of the site in ways which reinforce Washington Street as Yountville's main commercial street;
- 6 Signage: All signage shall comply with Section 6.3 of the Zoning Ordinance; and
- 7 Design Review Special Requirements: Story poles or models which illustrate the 3-dimensional massing and architectural character of the proposed new buildings, including adjacent existing buildings, may be required in addition to the requirements established in Section 7.3, Design Review, of the Zoning Ordinance if in the opinion of the ZDRB such information is necessary to evaluate the impact.

D.3. Combined Commercial and Residential Buildings

There are several types of buildings which historically have accommodated the combination of commercial and residential uses. It is intended that regulating documents provide flexibility and include various combinations of working with dwelling. These types of buildings could enrich the town and accommodate the diverse needs of its citizens. The permitted locations for the combined use is as established in Section 4.8 and 4.9 of the Zoning Ordinance, and is limited so that commercial usage does not usurp housing or encroach on residential neighborhoods. The building types include: housing over commercial space; commercial buildings combined with detached residential buildings on the same parcel; and live/work buildings in which the portion of the building devoted to work exceeds 25 percent.

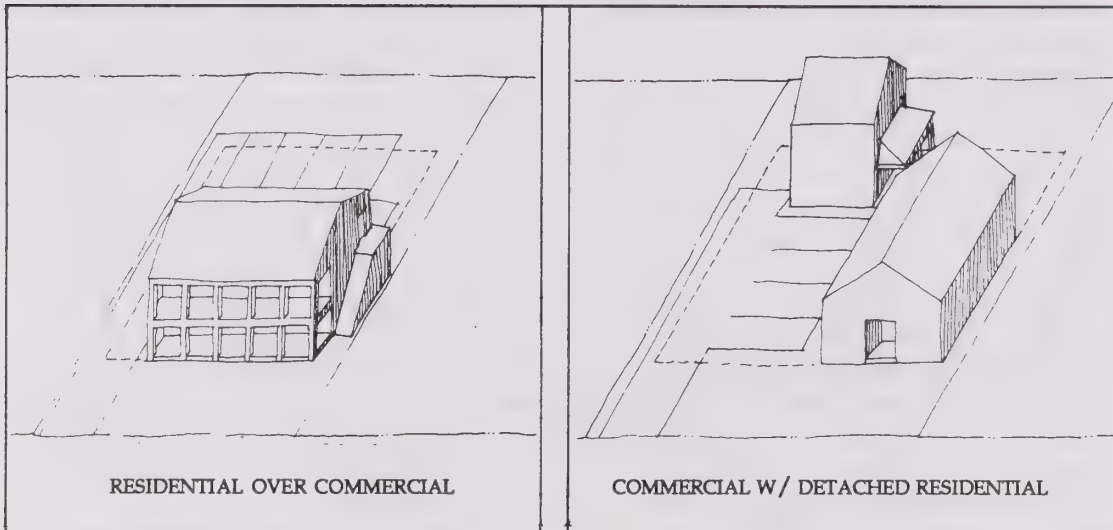


Figure II.7 Examples of Combined Commercial and Residential Buildings

D.3.a

Design Standards

The following design standards apply to new development throughout the town:

- 1 Floor Area Ratio: The maximum permitted Floor Area Ratio shall be 0.25 FAR. An additional 0.05 FAR for residential uses such as a second residential unit or an apartment is allowed. Exemption includes up to 250 square feet of unenclosed porches, balconies, attached carports, breezeways, and similar architectural elements.
- 2 Building Height: The building height shall not exceed two stories. One-story buildings shall not exceed 15 feet to the plate height or an overall height of 20 feet. Two-story buildings shall not exceed 22 feet to the plate height or an overall height of 30 feet. No more than one third of the total permitted FAR for a commercial development may occupy the second level;
- 3 Open Space: A minimum of 25 percent of the gross area of a given parcel shall be provided as usable open space. The open space requirement may be met by providing a series of connected smaller areas which have a minimum area of 400 square feet each. Residential uses shall have no less than 100 square feet of private open space for each unit, half of which shall be directly adjacent to its unit;
- 4 Parking: Parking ratios shall be as established in Section 6.2 of the Zoning Ordinance. Parking lots shall be adequately screened, and comply with design standards established in Section A.2 of this document;
- 5 Signage: All signage shall comply with Section 6.3 of the Zoning Ordinance; and

- 6 Design Review Special Requirements: Drawings, models, renderings, or perspectives which illustrate the 3-dimensional massing and architectural character of proposed new buildings, including adjacent existing buildings, shall be required, in addition to the requirements established in Section 7.3, Design Review, of the Zoning Ordinance.

E. Accessory Buildings and Facilities

Non-habitable buildings and structures which are subordinate and complementary to the primary use, and separate from dwelling units or commercial buildings shall meet the following design standards. Landscape elements, such as trellises, patios, decks below 30 inches, and arbors are not considered accessory structures, and therefore, are not limited by this section.

E.1.a Design Standards

The following design standards apply to new development throughout the town:

- 1 An accessory structure shall not be permitted in the front or side yards or setbacks, except for encroachments allowed in Chapter I.C.3.b.
- 2 An accessory structure shall not exceed one story or 10 feet to the plate height or an overall maximum height of 15 feet, with an exception for garages. Where necessary to enhance the architectural design, garages shall not exceed an overall maximum height of 20 feet. For the permitted building height of second units over garages refer to Section C.4 of this chapter ;
- 3 Accessory structures shall have materials consistent with primary buildings;
- 4 Accessory structures shall be limited to garages, carports, patio covers, swimming pools, hot tubs or spas and related equipment, decks over 30 inches above ground, workshops, storage sheds, gazebos, and greenhouses. Other similar structures may be permitted as determined by the Town Council;
- 5 Adjoining properties may share an accessory structure which serves as a garage provided that there is a joint-use agreement and the accessory structure straddles the shared property line;
- 6 Carports must be located in the rear half of the lot or screened from view from the street;
- 7 Only accessory structures which meet the provisions as established in Section 5.1 of the Zoning Ordinance and Section C.4 of this chapter shall be permitted to be used as a second residential unit; and
- 8 Setbacks and permitted encroachments for accessory structures shall be as established in Chapter I, Section C.3.

Design Document
Specific Parcels & Uses

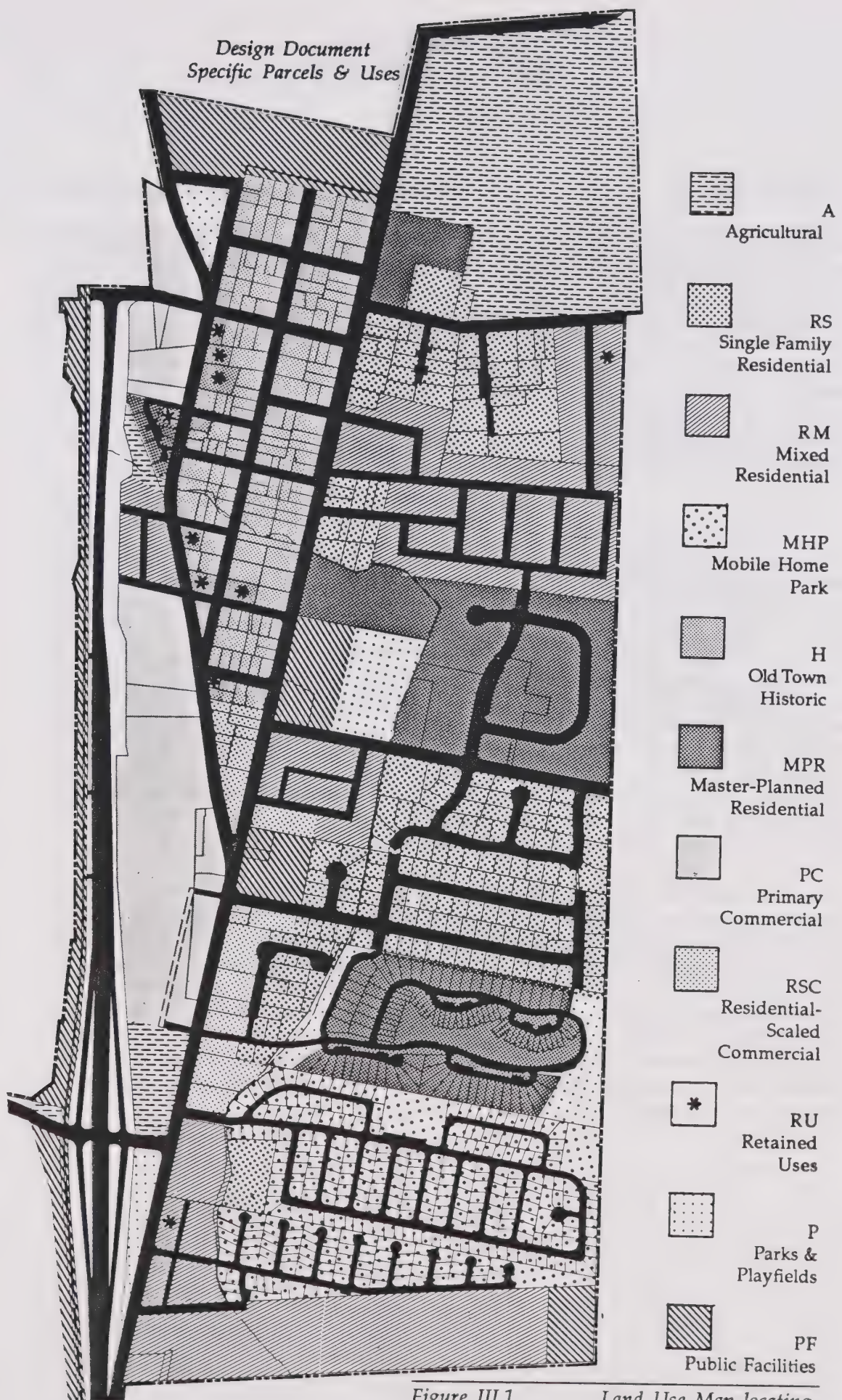


Figure III.1

Land Use Map locating
"Retained Uses"

CHAPTER III.

SPECIFIC PARCELS & USES

The General Plan identifies parcels within the town where more detailed regulations are necessary to address a particular condition or to ensure a specific intent. These parcels fall into one of two categories: those parcels occupied by commercial uses the Town wishes to maintain in a controlled way; and those parcels which are undeveloped. The Land Use Map, shown on the left, locates parcels designated as "Retained Uses", and in Figure III.2, Map of Unbuilt Parcels, the other parcels are identified. It is the intent of the design standards outlined below to ensure that existing businesses remain, and all new development reinforce the Town Structure as established in the General Plan. Modification, addition to, or re-construction of Retained Uses shall be permitted through the Master Development Plan process.

A.

RETAINED USES

The General Plan acknowledges the important contribution that many of the existing businesses make to the rural feeling of the town. Many of these businesses have been operating as legal non-conforming businesses as a result of their location within Old Town or other primarily residential neighborhoods. The building types vary, and include residential-scaled buildings used for a market, a beauty salon, a bar, or a restaurant, and larger buildings used for a car repair garage and a building materials supply business. To ensure that these businesses remain and their respective properties not revert entirely to residential use, the General Plan and Zoning Ordinance establish an overlay designation for these businesses called Retained Uses. Specific requirements for Retained Uses are contained in Section 5.5 of the Zoning Ordinance. New residential uses are encouraged in addition to the existing commercial use, provided that any new building is compatible with the scale and character of buildings on adjacent properties, and FAR requirements can be met.

The design standards outlined on the following pages will regulate Retained Uses only when the existing use comes under review for an amended or new Use Permit, and are intended to address specific issues and buildings for each property where the overlay designation, Retained Uses, has been identified. Although expansion of Retained Uses is permitted, there is a distinction between major and minor, and in no case will the additional square footage exceed the permitted FAR. Expansion resulting in an increase of greater than 25 percent of the existing square footage will be considered major; and 25 percent or less, minor. In the event of fire or other significant destruction of existing buildings or structures, all designated Retained Uses are permitted to rebuild within their existing footprints.

A . 1

APN 36-033-01 (Yountville Market)

Yountville Market, located at the northwest end of town, provides select food items and deli service for the town's residents and visitors. The existing building fronts onto Washington Street, and is architecturally important to Yountville's small town character. In the event that its current use is discontinued and it is converted to another commercial use, the building should be retained. Any new use will not alter the building in ways which would damage its residential scale and character.

A . 1 . a

Design Standards

The following design standards apply to APN 36-033-01 (Yountville Market):

- 1 Primary Building: The existing building and its front porch shall not be permitted to be demolished, and shall be preserved and maintained;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional square footage does not exceed .25 FAR and is recorded on a Use Permit. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit.

A . 2

APN 36-033-15 (Pancha's Bar)

Pancha's Bar is considered a distinctive local establishment. Although the existing building is not architecturally significant, it does have an important location within the town and provides continuity to the street frontage along Washington Street. In the event the property owner desires to replace the existing building or change the existing commercial use, there are design issues which need to be addressed. The design standards outlined below are intended to direct change in ways which maintain and reinforce Washington Street as Yountville's commercial main street.

A . 2 . a

Design Standards

The following design standards apply to APN 36-033-15 (Pancha's Bar):

- 1 Primary Building: The existing building may be replaced, but only with a new building which is similar in size and character. Also, in the event the existing building is replaced, the new building be residential in scale and shall provide a front porch on Washington Street with a street-oriented entrance;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. When only one unit is proposed with a minimum unit size of 600 square feet, a FAR exception may be granted. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit. In addition to the commercial use, it is permitted to provide new residential uses in the rear of the lot and above commercial uses fronting Washington Street. All new residential development shall follow the provisions established in Section 4.4 of the Zoning Ordinance for Mixed Residential. All new uses shall be compatible with adjacent parcels.

A . 3

A P N 36-033-14 (McGrath's Garage)

McGrath's Garage provides a unique service for the town's residents; car repair and maintenance. Although the existing building is not architecturally significant, it does have an important location within the town and provides continuity to the street frontage along Washington Street. Also located on the property is a small professional office building which is currently used for the offices of a balloon company but has had many different businesses in the past. In the event the property owner desires to replace the existing building or change the existing commercial use, there are design issues which need to be addressed. The design standards are intended to direct change in ways which reinforce Washington Street as the town's commercial main street.

A . 3 . a

D e s i g n S t a n d a r d s

The following design standards apply to APN 36-033-14 (McGrath's Garage):

- 1 Primary Building: The existing building may be replaced, however, the new building shall be residential in scale and provide a front porch on Washington Street with a street-oriented entrance;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. When only one unit is proposed with a minimum unit size of 600 square feet, a FAR exception may be granted. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit. In addition to the commercial use, it is permitted to provide new residential uses in the rear of the lot and above commercial uses fronting Washington Street. All new residential development shall follow the provisions established in Section 4.4 of the Zoning Ordinance for Mixed Residential. All new uses shall be compatible with adjacent parcels.

A.4 APN 36-090-05 (Napa Valley Wine Soap, Napa Valley Express,
The Massage Place & Debbie's Beauty Salon)

Napa Valley Wine Soap, Napa Valley Express, The Massage Place and Debbie's Beauty Salon are existing businesses located at the southwestern end of town along Washington Street. Napa Valley Wine Soap and Napa Valley Express occupy a small house-like building and provide copying/communication related services to residents as well as sell its wine soaps. The Massage Place and Debbie's Beauty Salon share a similar type building next door, and provide various beauty related services and massage. These buildings are residential in scale, and compatible with neighboring houses. The design standards outlined below are intended to ensure that any changes in use provide similar benefit to residents as the existing businesses, and reinforce Washington Street as Yountville's main commercial street.

A.4.a Design Standards

The following design standards apply to APN 36-090- 05 (Napa Valley Wine Soap, Napa Valley Express, The Massage Place & Debbie's Beauty Salon):

- 1 Primary Buildings: The existing buildings may be replaced, however, any new buildings shall be residential in scale as established by Section 4.9 of the Zoning Ordinance, provide street-oriented entrances, and have at least one architectural element per building as established in Chapter II, Section B.2;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. When only one unit is proposed with a minimum unit size of 600 square feet, a FAR exception may be granted. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit. In addition to the commercial use, it is permitted to provide new residential uses in the rear of the lot and above commercial uses fronting Washington Street. All new residential development shall follow the provisions established in Section 4.4 of the Zoning Ordinance for Mixed Residential. All new uses shall be compatible with adjacent parcels.

A . 5 A P N 3 6 - 0 4 0 - 1 1 (K n i g h t L u m b e r C o m p a n y)

Knight Lumber Company, located on the northeastern boundary of town, provides important building and agricultural supplies for local residents as well as people in the surrounding region. The barn-like main building signifies Yountville's agricultural past, and in many ways, Knight's serves as a general store similar to those historically found in rural small towns. The design standards outlined below are intended to help maintain Knight's as a viable business which serves the community without adversely affecting its residential neighbors. The General Plan and Zoning Ordinance designate the entire parcel as Mixed Residential with an overlay Retained Use. Therefore, the portion of this property not currently utilized for the existing business shall be residential in use, when developed.

A . 5 . a D e s i g n S t a n d a r d s

The following design standards apply to APN 36-040- 11 (Knight Lumber Company):

- 1 Primary Building: Changes in the form of the existing barn-like building shall not be permitted. In the event the owner desires to replace the existing building, the new building shall be of similar style and character. The new building's scale and design should give the appearance of multiple buildings, not one massive building;
- 2 Accessory Buildings or Structures: Modifications to existing accessory buildings or structures shall be permitted and include combining and re-configuring. In the event any of the existing buildings or structures are replaced, no new building or structure shall exceed the size or scale of the primary building;
- 3 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Major expansion shall be reviewed by the Town Council;
- 4 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 5 Use: In the event that the Retained Use is discontinued, a change in commercial use would require an amendment to the General Plan. Since the parcel is designated as Mixed Residential, a replacement use of residential shall not require an amendment. Expansion of existing commercial use includes areas needed to accommodate general merchandise, gardening supplies, small nursery, or similar products related to the existing business.

A . 6

APN 36-033-04 (Webber Place)

Webber Place is an established bed and breakfast inn located on Webber Street in Old Town. This building has been and remains important to the town's character. There is an existing barn which adds to the character of the property and is currently used for storage.

A . 6 . a

Design Standards

The following design standards apply to APN 36-033-04 (Webber Place):

- 1 Primary Building: The existing building shall be permitted to be altered provided that the changes are not detrimental to its house-like character, and the unenclosed front porch is preserved and maintained;
- 2 Barn Building: Another significant building on the property is the existing barn which should be encouraged to be renovated for other uses;
- 3 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. When only one unit is proposed with a minimum unit size of 600 square feet, a FAR exception may be granted. Major expansion shall be reviewed by the Town Council;
- 4 Parking: There are no parking spaces provided for the existing use, however, any changes in use shall provide adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 5 Use: Any change in use, except conversion of the existing barn to accommodate expansion of the existing use, shall require an amendment to the General Plan and shall meet the provisions established during that process.

A . 7

A P N 36-051-03 (B o r d e a u x H o u s e)

Bordeaux House fronts Washington Street on the north end of town. Currently, the property is occupied by several buildings, including a house, an inn, and accessory buildings which serve the inn for storage and laundry facilities. At the same time the bed and breakfast was completed, the existing house was renovated and must remain a private residence. Bordeaux House provides a good existing example of combined commercial and residential uses.

A . 7 . a

D e s i g n S t a n d a r d s

The following design standards apply to APN 36-051-03 (Bordeaux House):

- 1 Primary Building: The existing building shall be permitted to be altered provided that the changes are not detrimental to its house-like character;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit.

A . 8

APN 36-051-05 (French Laundry)

The French Laundry is an established restaurant located at the northwest end of town on Washington Street. The existing buildings are architecturally important to the town's character and have served a variety of uses in the past. The scale of the buildings serve as a transition from the commercial uses along Washington Street to residential part of Old Town. Major features of the property include a significant garden adjacent to the house-like building, and a well-landscaped gravel parking lot for patrons. In the event that the current use is discontinued and converted another commercial use, the existing main building and its garden should be retained. The design standards outlined below are intended to address change in the existing use or building, and to ensure that the use of this property reinforces the Town Structure.

A . 8 . a

Design Standards

The following design standards apply to APN 36-051-05 (French Laundry):

- 1 Primary Building: The existing building and use shall not be permitted to be demolished, and shall be preserved and maintained. Any new use would not be allowed to alter the main building in ways which would be detrimental to its residential scale and character;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Major expansion shall be reviewed by the Town Council. Expansion shall be limited to that which does not result in additional seating, such as improvements or upgrades, or the need for additional parking spaces;
- 3 Parking: The existing parking spaces and lot shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance;
- 4 Use: Any change in use shall be required to meet the provisions as established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit;
- 5 Setbacks: The existing buildings hug the street edge of Creek and Washington Streets. In the event of fire or other significant destruction of existing buildings, the replacement buildings shall be permitted to rebuild in the existing footprints, even though, the current setbacks do not meet the established minimums for Old Town Historic neighborhood; and
- 6 Garden: The existing garden should be preserved and maintained as open space. The intent is to preserve the garden from being usurped to provide more parking spaces.

A.9 APN 36-061-15 (Former U.S. Post Office)

A small building, formerly occupied by the U.S. Post Office, fronts onto Washington Street near the center of town. Its location is important to the continuity of Washington Street as Yountville's commercial main street, and therefore, has been identified to be retained as a commercial use. The building was recently renovated and transformed to accommodate an art gallery and framing shop. The new architectural style is similar to Mediterranean buildings and enhances Yountville's mix of styles. The design standards outlined below address the importance of the building to the Town Structure.

A.9.a

Design Standards

The following design standards apply to APN 36-061-15 (Former U.S. Post Office):

- 1 Primary Building: The existing building may be replaced, however, any new building shall be residential in scale as established in Section 4.8 of the Zoning Ordinance and provide street-oriented entrances. Alterations to the existing building shall be permitted provided the changes are not detrimental to its house-like character;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Where the expansion includes residential square footage, the FAR shall not exceed .30 FAR. Major expansion shall be reviewed by the Town Council;
- 3 Parking: There are no parking spaces for the existing use and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance; and
- 4 Use: Any change in use shall be required to meet the provisions, with the exception of the parking requirement, as established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining a Use Permit.

*Design Ordinance
Specific Parcels and Uses*

A . 1 0

A P N 3 6 - 4 4 0 - 0 1 (B u r g u n d y H o u s e)

Burgundy House is part of Yountville's agricultural small town heritage. Originally a house with a small business, it has subsequently accommodated a shop selling antiques with a hotel on the second level. In the recent past and currently, the building serves as a well-established bed and breakfast inn. As an acknowledgment of its importance to the towns' character, the building and its use were retained in the recent development of Burgundy Commons. The design standards outlined below are intended to reinforce Burgundy House as an integral part of the town and define parameters for its continued use within the existing building.

A . 1 0 . a

D e s i g n S t a n d a r d s

The following design standards apply to APN 36-440-01 (Burgundy House):

- 1 Primary Building: The existing building and its front porch shall not be permitted to be demolished, and shall be preserved and maintained;
- 2 FAR and Expansion: Minor or major expansion to the existing commercial use shall be permitted provided that the additional commercial square footage does not exceed .25 FAR and is recorded on a Use Permit. Major expansion shall be reviewed by the Town Council;
- 3 Parking: The existing parking spaces shall be maintained and any changes in use shall provide for adequate parking. Where expansion occurs, adequate parking shall be provided for that portion of the expansion as established in Section 6.2 of the Zoning Ordinance;
- 4 Use: Changes in use dissimilar to those in the past shall be required to meet the provisions as established in Section 4.9 of the Zoning Ordinance for Residential-scaled Commercial which includes obtaining an Use Permit; and
- 5 Setbacks: The existing front yard shall be preserved and maintained.

Design Document
Specific Parcels & Uses



A
APN 36-040-10
(Mayeri)

B
APN 36-040-11
(Knight)

C
APN 36-040-13
(Lande)

D
APN 36-040-12
APN 36-070-14
(Forrester)

E
APN 36-330-01
(Filippi)

F
APN 36-090-23
APN 36-090-22
(Bardessono)

G
APN 36-081-08
(Carbone)

H
APN 36-081-10
(Byrnes)

I
APN 36-330-07
(Vintage
Partners)

J
APN 36-082-08
APN 36-082-09
(Altamura)

K
APN 36-090-26
(Altamura)

L
APN 36-090-05
(Rabe)

M
APN 36-090-02
(Catholic
Church)

N
APN 36-361-01
(Patton)

Figure III.2

Map of Unbuilt Parcels

B .

UNBUILT PARCELS

The remaining unbuilt parcels within Yountville comprise a significant amount of land and when developed will have an impact on the character of the town. The General Plan discusses their potential development, and the Zoning Ordinance designates the land use for each parcel with related procedures necessary for approval of proposed new development. However, there are other issues specific to each parcel which require consideration during the review process. These issues are outlined below according to parcel number. The intent of identifying these issues is to ensure that all new development is completed in ways which reinforce the character of the town and its structure. The map shown in Figure III.2 on the left identifies these parcels.

B . 1 .

A P N 36-090-26 (A l t a m u r a)

There are two issues which require consideration during the review of the proposed development in addition to those established for Mixed Residential in the Zoning Ordinance. Situated at the southern end of town where California Drive terminates into Washington Street, this parcel provides an important first impression of the town's character. The proposed development should serve to define this entrance into town in a way that is compatible with its established rural feeling. A possible solution would provide a swath of open space as a visual extension of California Drive. At the terminus of California Drive, residential buildings should be placed so as to avoid direct focus of the headlights of cars. Additionally, the lots fronting onto Washington Street should have car access from an interior street to minimize curb cuts along Washington Street and avoid a potential increase in traffic congestion. However, the residential buildings on these lots should have their main entrance front onto Washington Street.

B . 2 .

A P N 36-090-22 & 36-090-23 (B a r d e s s o n o)

As a result of its location at the corner of Finnell Road and Yount Street, the street frontage of this parcel requires special consideration in addition to the provisions established for Residential-scaled Commercial and Mixed Residential in the Zoning Ordinance. Sandwiched between community facilities to the north and south, residential development to the east, and Old Town to the west, this parcel has been identified to serve as a transition from the commercial area to the residential neighborhoods. Therefore, a portion of the property has been designated for residential-scaled commercial uses adjacent to the Community Hall along its southwestern boundary. To offset a potential increase in traffic congestion, curb cuts along Yount Street and Finnell Road should be minimized and the sharing of the existing access drive on the southern boundary should be considered for new commercial buildings. Residential lots fronting onto Yount Street and Finnell Road should have car access from an interior street or alley, however, the residential buildings on these lots should have their main entrance on Yount Street and Finnell Road.

In addition to this parcel serving as a transition it also will serve as a link for the local pedestrian and bike path. The Yountville Bike Plan proposes a path for the shared use of pedestrian and bicyclists along the parcel's eastern boundary adjacent to Hopper Creek.

*Design Ordinance
Specific Parcels and Uses*

B . 3 .

A P N 36-081-10 (B r y n e s)

There are issues regarding adjacent parcels and the frontage along Washington Street for this parcel in addition to the regulations for the preservation of existing trees established in Chapter I, Section B.5, and those established for Primary Commercial in the Zoning Ordinance. As a result of its irregular configuration this parcel should be permitted to have one curb cut on Washington Street to provide primary access. During the review of proposed development for this parcel or adjacent properties the joint use of parking and access drives should be considered to promote interconnectedness and continuity of the town.

B . 4 .

A P N 36-081-08 (C a r b o n e)

Although there are no special regulations related to this parcel beyond those established for Primary Commercial in the Zoning Ordinance, there is one issue which requires attention during the review of proposed new development. Located at the corner of Washington and Mulberry Streets, the unbuilt area is the remaining portion of a larger parcel. Access to parking for all proposed commercial uses should be provided from Mulberry Street, and no curb cut should be permitted along Washington Street.

B . 5 .

A P N 36-090-02 (C a t h o l i c C h u r c h)

Bisected by Hopper Creek, this large parcel has significant constraints related to its location within the floodplain which are addressed in Chapter IV of the General Plan, and Section 5.4 and 6.9 of the Zoning Ordinance. There are additional criteria outlined in Chapter I, Section B.3 for the Hopper Creek setback area. As a result of these issues the street configuration has not been established for this parcel, however, Chapter I, Section B.2 discusses the intent of providing new north-south view corridors from within the property to the surrounding landscape.

B . 6 .

A P N 36-330-01 (F i l i p p i)

In addition to the regulations established for Mixed Residential in the Zoning Ordinance, there are issues related to the frontage of this irregular-shaped parcel which is situated between Washington Street and Highway 29. The General Plan shows the configuration of new streets for the parcel which includes extending Creek Street and connecting to Webber Street. However, to avoid creating a left-turn hazard on Washington Street or increasing traffic congestion, the extension of Creek Street to the west may need to be restricted to allow right-turns only. Special study of this intersection should be included in review of proposed new development for this parcel. Also, the front setback and location of development may be further restricted along the Washington Street frontage as a result of an utilities easement for the water service.

Proposed development along the parcel's frontage on Highway 29 should consider the view from the roadway to the town, as well as the impact of the noise. Also adjacent to the Highway, the Yountville Bike Plan proposes a path, however, an alternative solution which connects the path from Hopper Creek to Vintage Inn along New Street 'B' should be considered.

B . 7 . A P N 36-040-12 /36-070-14 (F o r r e s t e r)

There are issues related to the street layout for this parcel in addition to the regulations established for Mixed Residential in the Zoning Ordinance and for open space in Chapter I, Section B.1-5 (which include providing new open space, maintaining existing view corridors and creating new ones, and preserving existing trees and frontage along Hopper Creek). The Street Identification Map in Chapter I, Figure I.2 shows the proposed configuration of new streets within this parcel. Although the degree of flexibility is outlined for each street in Chapter I, Section A.1, there are principles which guided the layout that should be considered in the review of new development. As part of the principles of gridiron planning there is a deliberate intention to connect existing streets throughout the town, and therefore, the proposed new streets within this parcel serve to link Old Town, Toyon Terrace and Knight Lumber Company to each other and other parts of town. However, in response to the expressed concern of residents regarding safety and high speed through-traffic, the layout establishes T-intersections for the incoming streets to the parcel- Mount Avenue, Vista Drive, proposed Stags View Lane. Review of proposed new development should also consider the need for additional safety measures, such as varied paving materials or special signage at pedestrian crossings.

B . 8 . A P N 36-040-13 (L a n d e)

Review of proposed new development should consider the street frontage along Yount Street in addition to the provisions for Mixed Residential in the Zoning Ordinance, and regulations for new streets in Chapter I, Section A.1, and view corridors in Chapter I, Section B.2. Where possible along Yount Street, curb cuts should be minimized and car access to lots fronting Yount Street be provided from within the parcel. Entrances to houses should front onto Yount Street. Also, the through-connection of Street 'C' to Street 'D' is important to the interconnectedness of the town.

B . 9 . A P N 36-361-01 (P a t t o n)

Special consideration should be given to the configuration and placement of the building as a result of existing trees, in addition to the regulations established Residential-scaled Commercial in the Zoning Ordinance, and for preserving existing trees in Chapter I, Section B.5. To minimize curb cut along Washington Street and avoid a potential increase in traffic congestion access to all parking spaces should be provided from Oak Circle. However, the residential-scaled commercial buildings should have their main entrance front onto Washington Street.

B . 10 . A P N 36-090-05 (R a b e)

Special consideration should be given to the street frontage of this parcel in addition to the regulations for Mixed Residential and for the Hopper Creek setback area established in Chapter I, Section B.3. The lots fronting onto Washington Street should have car access from an interior street or alley to minimize curb cut along Washington Street and avoid a possible increase in traffic congestion. However, the residential buildings on these lots should have their main entrance front onto Washington Street.

*Design Ordinance
Specific Parcels and Uses*

B . 1 1 .

A P N 3 6 - 3 3 0 - 0 7 (V i n t a g e P a r t n e r s)

There are issues related to the boundaries of this parcel which should be considered during the review of proposed new development in addition to provisions for Primary Commercial established in the Zoning Ordinance and regulations for providing Open Space in Chapter I, Section B.1. Proposed development along the parcel's western boundary, the Highway 29 frontage, should take in to account the view from the roadway to the town, as well as, the impact of noise. Where possible, trees and landscape should screen the view of parking lots from the highway.

The parcel's eastern boundary partially abuts Washington Street and partially abuts two other properties which front onto Washington Street. Since this parcel's frontage along Washington Street is limited, the internal configuration of streets is important for establishing a cohesive commercial area in relation to adjacent properties as well as its connections to Washington Street. Although the location of new streets is not specified as it is in other unbuilt parcels, the principles of gridiron planning which are the basis for the Town Structure should guide the ultimate configuration of any new streets within this parcel. The recommended street configuration is shown as dashed lines parallel to Washington Street in Figure I.2, Street Identification Map, which connects Mulberry Street and Oak Circle. Any new street has the opportunity to provide more than parking drive access by making pedestrian and vehicular connections to Washington Street. The intent is to encourage new commercial development which reinforces the continuity of Washington Street and contributes to an overall cohesiveness with adjacent properties; not disconnected independent developments with perimeter barriers which inhibit pedestrian or vehicular connections.

Throughout the discussion regarding this parcel it was the consensus that most of the parcel would be designated as Primary Commercial with a portion as Agricultural. With this assumption the permitted FAR will be calculated for the entire parcel, however, only on that portion designated as Primary Commercial shall be permitted to be developed with buildings. The portion designated as Agricultural shall be utilized for uses agricultural in nature only, and does not include buildings, parking or permanent storage.

*Design Ordinance
Adoption*

A D O P T I O N

NOW, THEREFORE, BE IT ORDAINED by the Town Council of the Town of Yountville, State of California, that Ordinance Number 232-92 is amended and said amendments incorporated in and become part of the original Ordinance.

THE FOREGOING ORDINANCE was introduced and received as a first reading at a regular meeting of the Town Council of the Town of Yountville held on the 25th day of January, 1994

IT WAS GIVEN a second reading, adopted and ordered posted at a meeting of the Town Council, Town of Yountville held on the 8th day of February, 1994 by the following roll call vote:

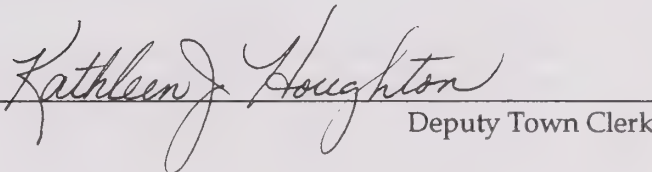
AYES:	Reeves, Houghton, Holt, Jefferson, Leftwich;
NOES:	None;
ABSENT:	None;
ABSTAIN:	None.

/s/


Carlee S. Leftwich, Mayor

ATTEST:

/s/


Deputy Town Clerk

End of Ordinance -----

CHAPTER IV.

APPENDICES

This chapter consists of a selection of Case Studies which were utilized in discussions regarding the content of this document, the Zoning Ordinance and General Plan. Also included are cross sections of a few streets in town which were compiled following a survey of the existing conditions. All of this information is for reference, and should not be considered binding.

A. CASE STUDIES FOR UNBUILT PARCELS

This section consists of the Case Studies made for most of the unbuilt parcels in Yountville. They reflect ideas consistent with gridiron town planning principles and building patterns found in the town. From these studies the street layout which reinforces the overall Town Structure was established. The individual lot layout and building types have been included for reference only, and are not mandatory. However, some of the principles and assumptions for each parcel are summarized on the following pages. Representative building types used in the Case Studies are shown below.

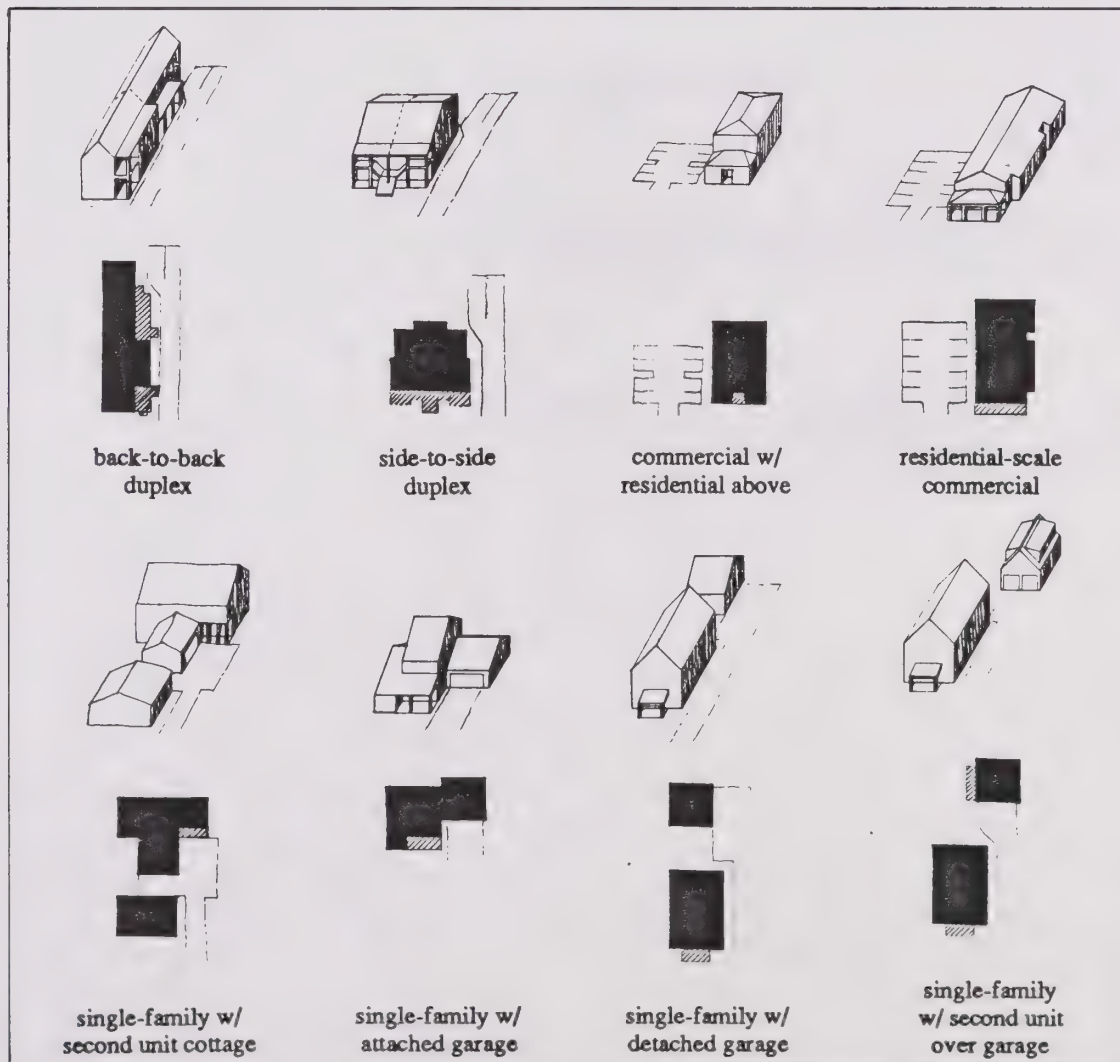


Figure IV.1

Building Types used in Case Studies

A . 1 .

A P N 36-090-26 (A l t a m u r a)

Located on Washington Street at the southern end of town, this 1.8-acre parcel serves as a gateway and is important to the identity and character of the town. The case studies show street-oriented dwelling units on lots ranging in size from 4,000 to 8,000 square feet. To maintain a sense of openness which currently exists upon entering the town, all the case studies show an open space area or street as an extension of California Drive from Washington Street to Hopper Creek. The Street Map does not designate a new street layout within this parcel, however, any new street should connect through to the Rabe property to the south and there should be only one additional curb cut on Washington Street.

CASE STUDY ONE

This scheme shows predominately single-family houses, with a few duplexes, on lots ranging in size from 4,000 to 8,000 square feet. Some houses would require driveways from Washington Street, however, most of the lots are accessed from a secondary street parallel to Washington Street and terminating into a cul-de-sac. The cul-de-sac is on axis to a swath of open space which is an extension of California Drive.

CASE STUDY TWO

This scheme shows predominately single-family houses with duplexes flanking the open space which terminates the extension of California Drive. Lots range in size from 4,000 to 8,000 square feet, and are accessed by the extension of California Drive or the intersecting secondary street parallel to Washington Street. A few houses require driveways from Washington Street.

CASE STUDY THREE

This scheme shows all duplexes on lots ranging in size from 5,000 to 8,000 square feet and accessed by a secondary street parallel to Washington Street. No driveway access from Washington Street would be necessary with this street and lot configuration.

CASE STUDY FOUR

This scheme shows all single-family houses on lots ranging in size from 5,000 to 8,000 square feet and accessed by an secondary street parallel to Washington Street. No driveway access from Washington Street would be necessary with this street and lot configuration.

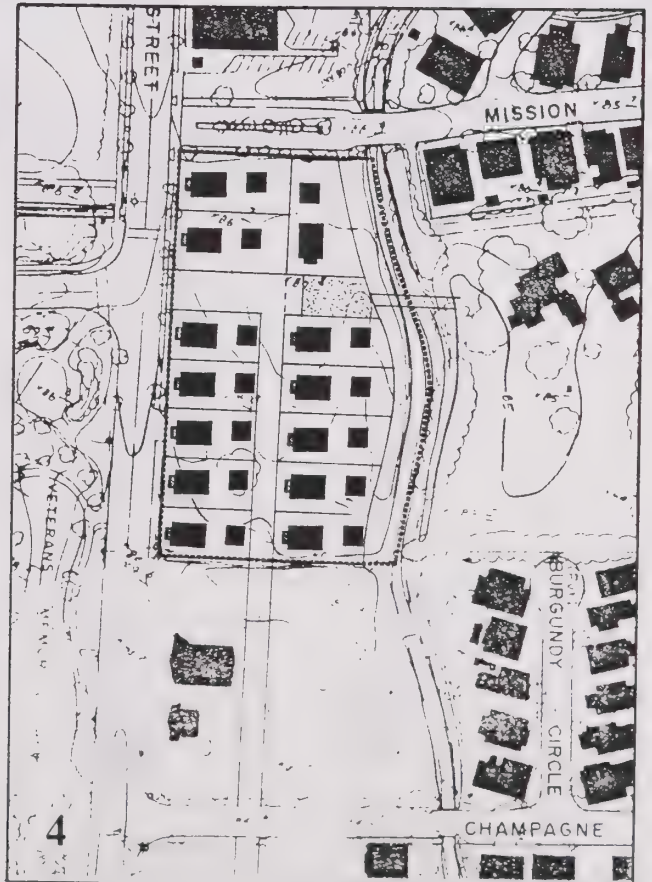
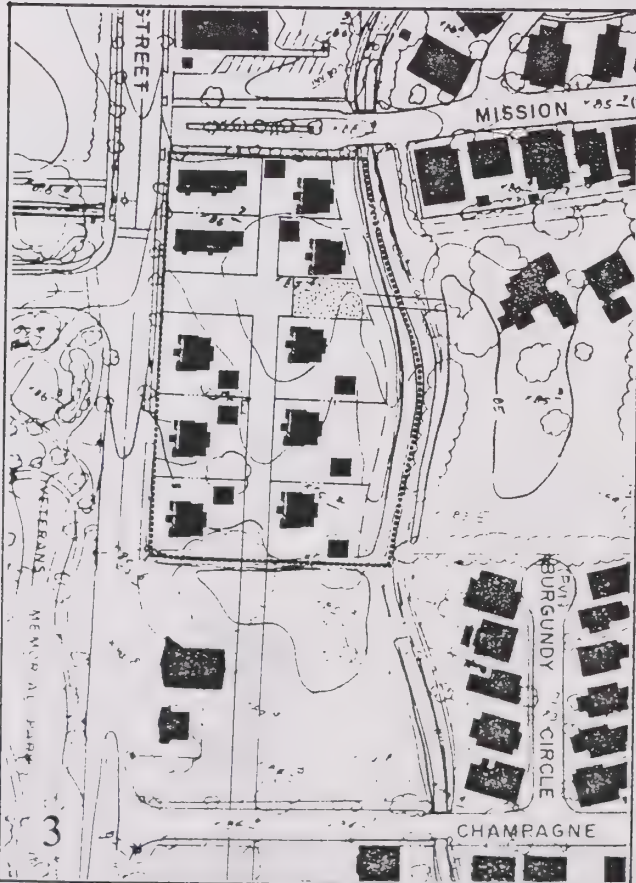
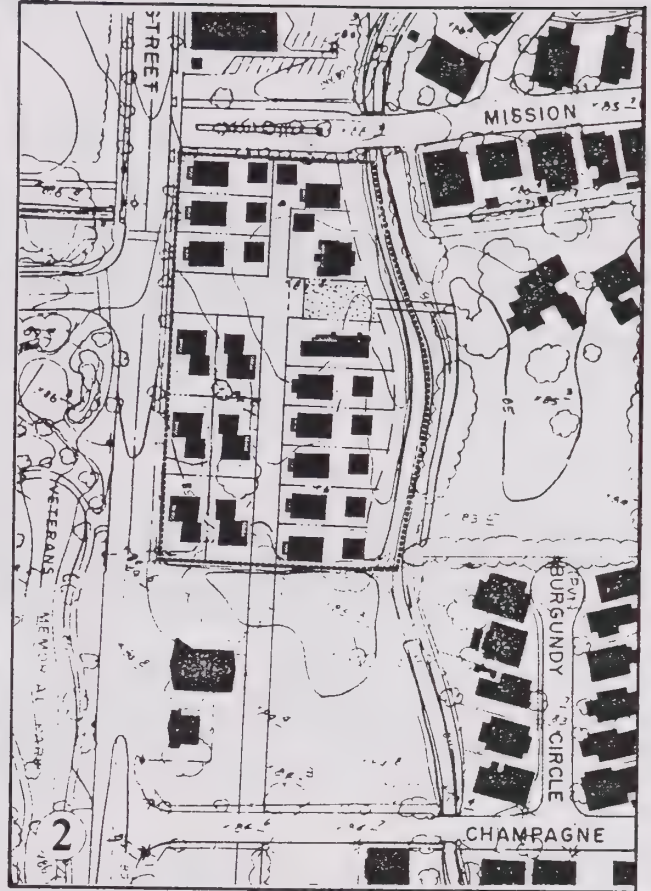


Figure IV.2

Altamura

A . 2 .

APN 36-090-22 & 23 (Bardessono)

Located at the corner of Yount Street and Finnell Road near the 'Y', these two parcels, which total 6.8 acres, will serve as a transition from the commercial main street, Washington Street, to the residential neighborhoods of Old Town and Heritage Estates. The case studies show variations of an L-shaped street configuration with the preferred layout shown in #3. The land use and street configuration of Case Study #3 reinforces the intent of the General Plan the most. It served as a reference for establishing related policies in the Zoning Ordinance and standards in the Design Document.

CASE STUDY ONE

This scheme shows a street layout which creates very efficient blocks with the residential lots ranging in size from 5,000 to 6,000 square feet. The residential buildings are predominately single-family houses with about one-third shown as back-to-back duplexes. Some houses would require driveways from Yount Street and Finnell Road which may result in additional traffic congestion.

CASE STUDY TWO

This scheme also shows a street layout which creates very efficient blocks with most of the residential lots between 5,000 and 6,000 square feet, but with an equal number of single-family houses as duplexes. There are about the same number of lots which would require driveways from Yount Street and Finnell Road. There is also a small parcel, about two-thirds of an acre, adjacent to Community Hall shown with two residential-scaled commercial buildings fronting Yount Street. This is considered to be a transition building type and use.

CASE STUDY THREE

This scheme shows the perimeter of the parcel lined with most of the residential lots ranging in size from 5,000 to 7,500 square feet and L-shaped primary street configuration with an secondary L-shaped residential alley. The lots fronting Yount Street and Finnell Road are accessed from the residential alley. There is also a similar size parcel as in #2 shown with residential-scaled commercial buildings.

CASE STUDY FOUR

This scheme has the same street layout, residential and commercial buildings types as shown in #2, but with portion of the parcel maintained as open space surrounding the existing house. The lot sizes are similar to #2. There are also about the same number of lots which would require driveways from Yount Street and Finnell Road. However, preserving the house was not viewed as essential.

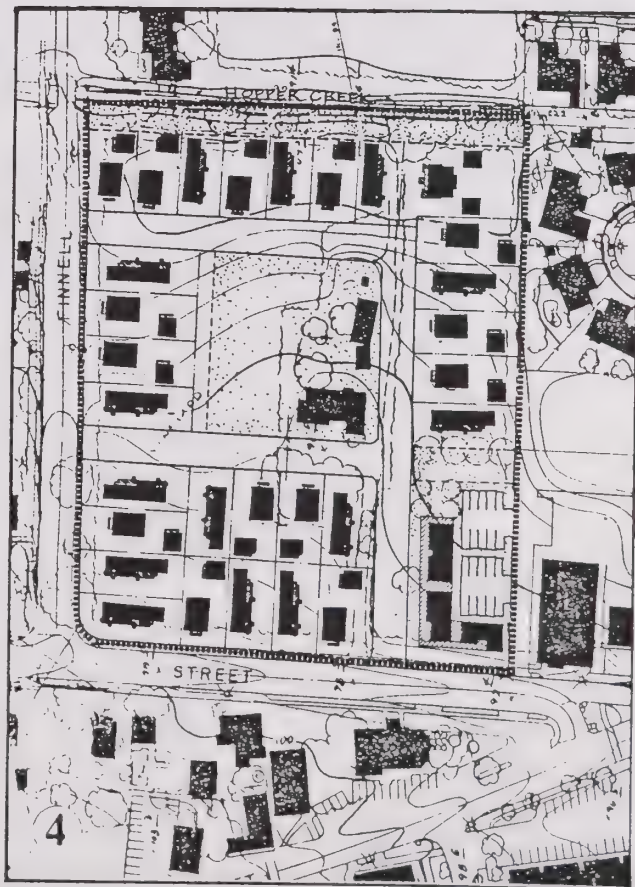
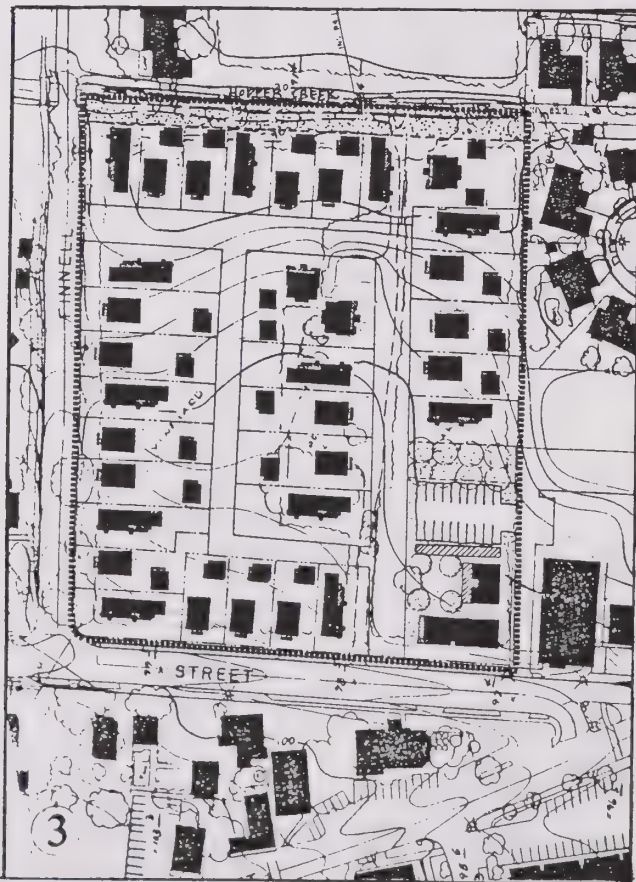
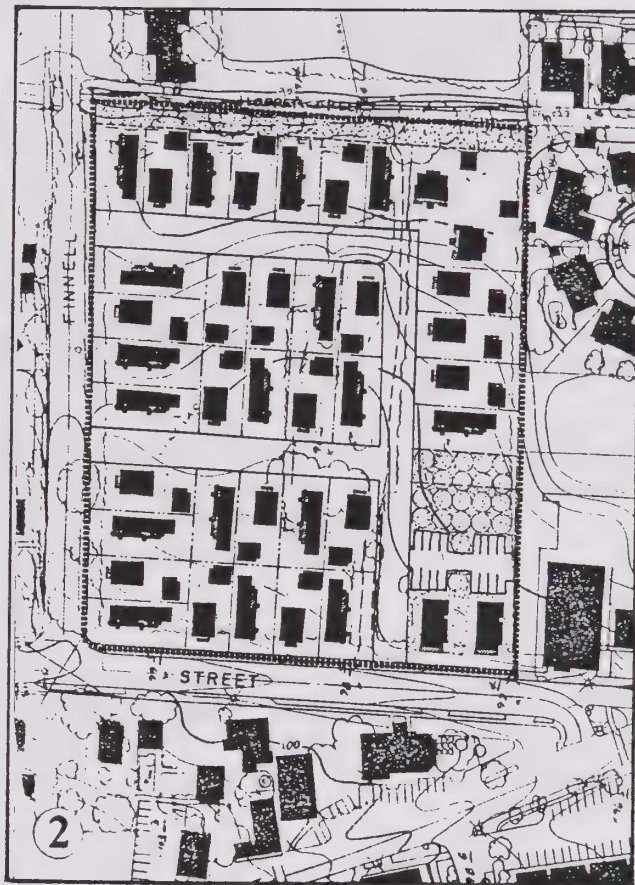


Figure IV.3

Bardessono

A.3.

APN 36-090-02 (Catholic Church)
& APN 36-090-05 (Rabe)

The two case studies shown below for the Catholic Church property do not illustrate viable alternatives but instead point out the complexities of this approximately 17-acre parcel related to its configuration and topography. This parcel will require substantially more study to develop and may require unit or building types other than those established in this document. On the other hand, the Rabe property is limited in ways that allow only a few possible solutions for the 2.2 acre parcel.

CASE STUDY ONE

For the Catholic Church property, this case study looked at a street grid similar in dimension to those found in Old Town and created a series of linear open spaces which could serve as a neighborhood park. All the new streets also could serve as view corridors to the surrounding landscape. This is an inefficient use of street frontage, however, as most of the streets have lots fronting on only one side of the street. For the Rabe property, the street layout of this case study provides alley access for all lots fronting onto Washington Street but does not enable a through-connection to the Altamura property to the north. Most of the residential buildings are single-family houses.

CASE STUDY TWO

For the Catholic Church property, this case study looked at a more efficient use of streets and created a boulevard-like one-way loop primary residential street intersected with cross streets terminating in courts serving four lots. The residential building types are predominately single-family houses with back-to-back duplexes on some the corner lots fronting the boulevard. This layout is very efficient with lot sizes ranging from 4,000 to 5,000 square feet leaving a significant portion of the parcel flanking the creek as open space. For the Rabe property, this is the most efficient street layout which shows lots ranging in size from 5,000 to 7,000 square feet with no new curb cuts needed from Washington Street.

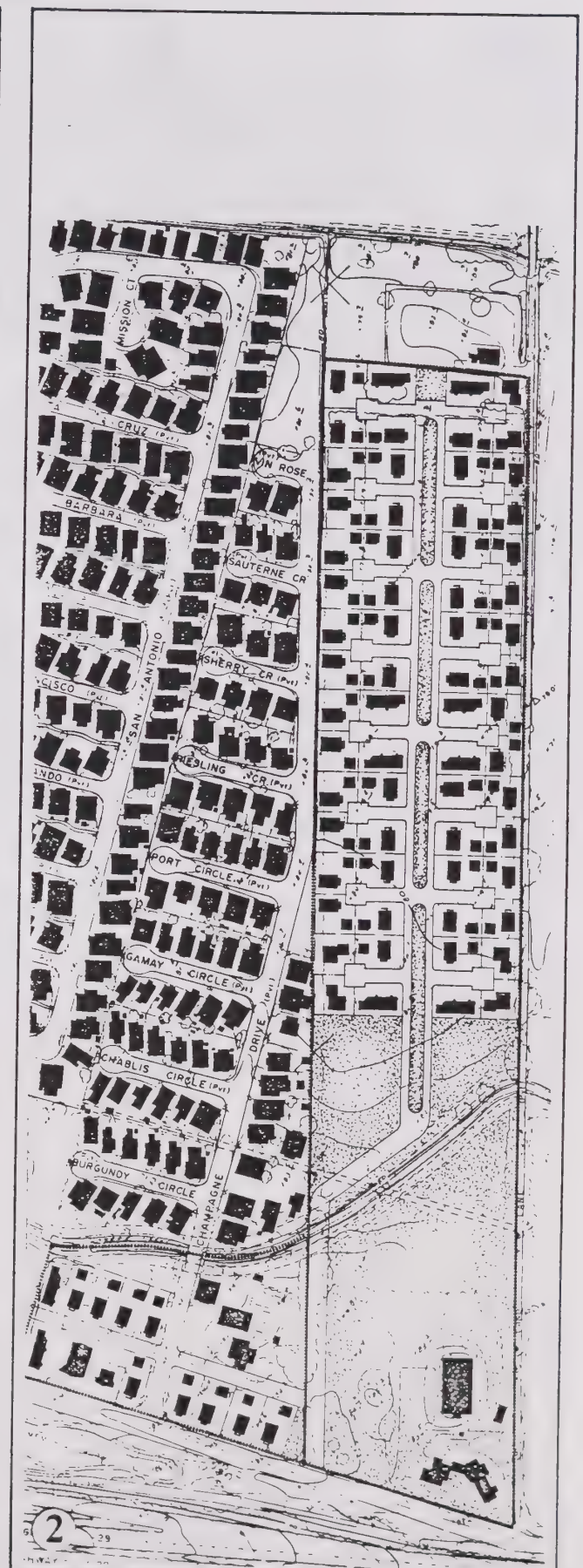
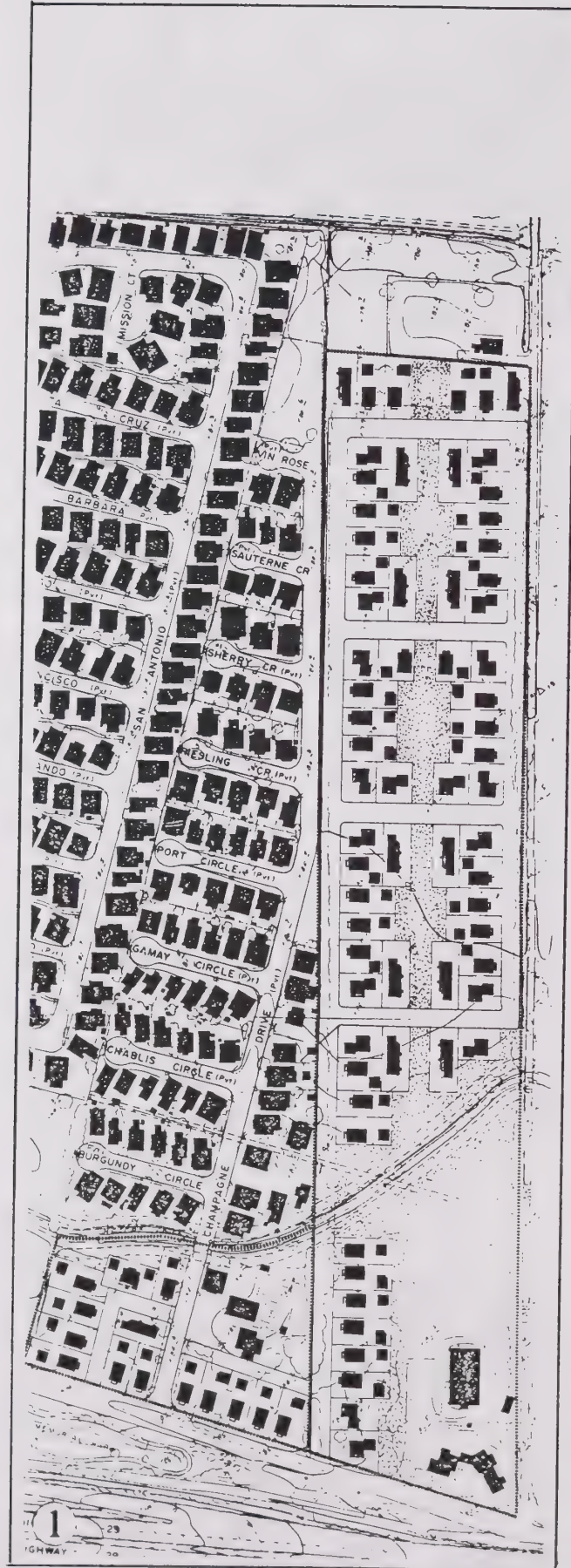


Figure IV.4

Catholic Church & Rabe

A . 4 .

APN 36-330-01 (Filippi)

Located at the northeastern end of town this 2.9-acre parcel is constricted by its odd shape and Hopper Creek. Therefore, the case studies show residential development of this parcel only on the portion south of Hopper Creek, with the portion to the north as open space. This property is further restricted by an utility easement along Washington Street which will require closer review for development along the street frontage. To maintain the existing view corridor to the west, the street layout for all the case studies show an extension of Creek Street across Washington Street. The lot and street configuration of Case Study #4 reinforce the intent of the General Plan the most. It served as a reference for establishing related policies in the Zoning Ordinance and standards in the Design Ordinance.

CASE STUDY ONE

This scheme shows a street layout which is very efficient with lots ranging in size from 5,000 to 10,000 square feet. The residential buildings are shown as all single-family houses, however, other multi-family types could be accommodated on most of these lots. The rear yards of lots fronting Creek and Webber Streets create a large open area adjacent to the highway.

CASE STUDY TWO

This scheme shows a through connection from the Creek Street extension to Webber Street with lots ranging in size from 5,500 to 10,000 square feet. There is a mix of single-family houses and back-to-back duplexes with a couple of fourplexes adjacent to the creek.

CASE STUDY THREE

This scheme shows a loop street configuration from Creek Street to Webber Street. The lot configuration would require an open space easement to ensure that Creek Street remains a view corridor. These lots range in size from 5,000 to 10,000 square feet.

CASE STUDY FOUR

This scheme shows lots ranging in size from 5,000 to 9,000 square feet with equal number of single-family houses and duplexes. The deep lots fronting onto Washington Street will provide flexibility to accommodate the restrictions of the utility easement upon further development and have the opportunity to be accessed from the new internal street.

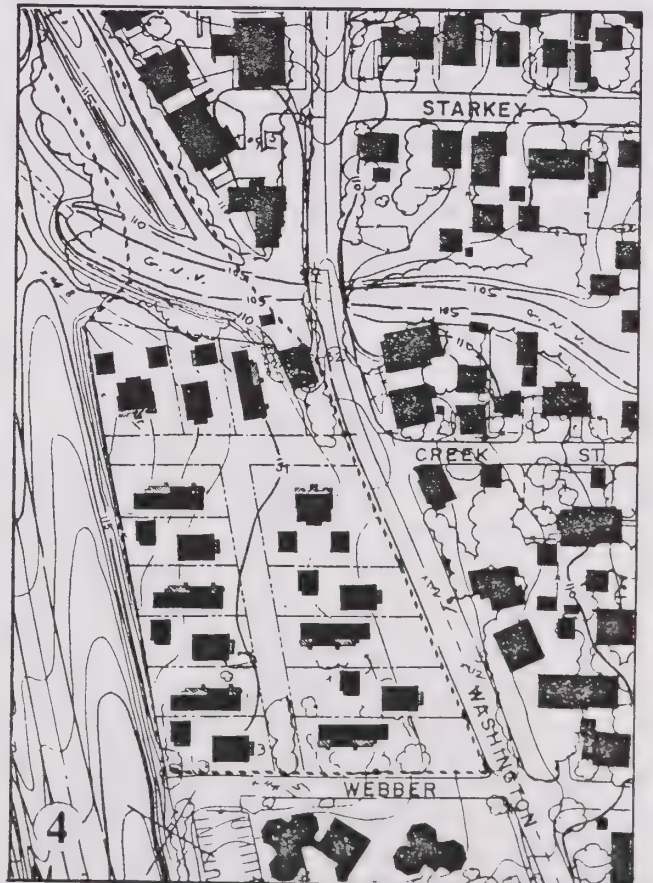


Figure IV.5

Filippi

A. 5.

APN 36-040-12 & 14 (Forrester)
& APN 36-040-13 (Lande)

These case studies, which included the Forrester and Lande parcels, looked at the combination of the largest amount of contiguous unbuilt land in the town. In all the case studies for the 15.9-acre Forrester property, the street layout creates blocks with similar dimensions of blocks in Old Town, maintains the Mount Avenue view corridor and connects to the surrounding developments. For the 4.7-acre Lande property the street layout serves to link the Forrester property and Yount Street.

CASE STUDY ONE

For the Forrester property, the street layout lots range in size from 5,000 square feet to 9,600 square feet and a mix of single-family houses with about one-third of the total as duplexes. For the Lande property, this shows an equal mix of single-family houses and duplexes with through-connection to Mesa Court.

CASE STUDY TWO

For the Forrester property this street layout is similar to #1 but shows a different configuration for potential open space which could serve as a neighborhood park. The residential lots range in size from 4,500 to 10,000 square feet with approximately 1 and one-third acre shown as open space. For the Lande property, this case study looked at maintaining most of the parcel in orchard surrounding the existing house and developing a portion of the property to the north with a connection to Mesa Court.

CASE STUDY THREE

For the Forrester property, this scheme was a refinement of Case Study # 1 which minimized the amount of land shown as open space, but maintained the view corridors and existing trees. For the Lande property, this scheme is similar to #2 but without a through-connection to the north.

CASE STUDY FOUR

For the Forrester property, this scheme was a refinement of Case Study # 2 which provided T-intersections for Vista Drive, Mount Avenue, and Stage View Lane. For the Lande property, this scheme is the most efficient layout of lots with sizes ranging from 5,000 to 8,000 square feet.

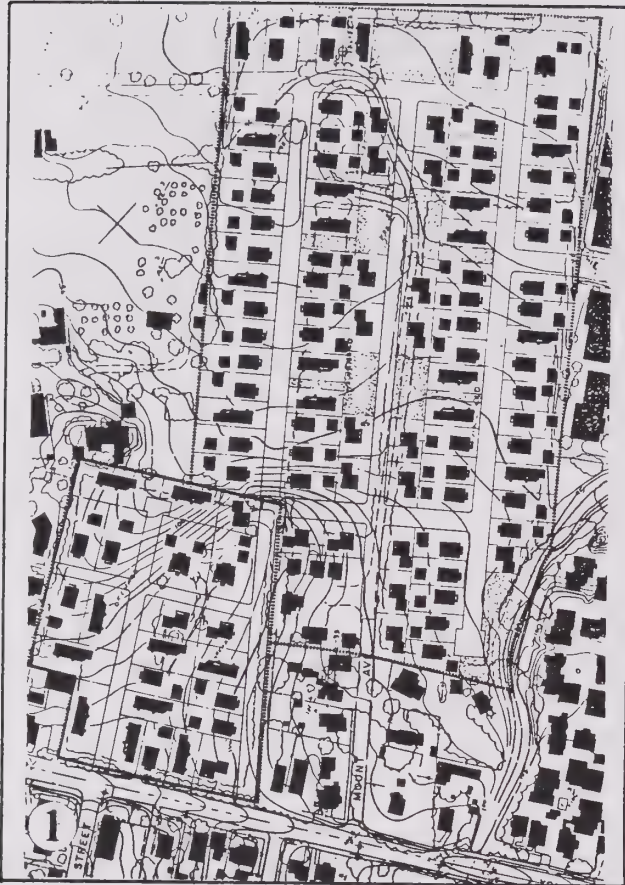


Figure IV.6

Forrester & Lande

A.6. APN 36-330-07 (Vintage Partners)
& APN 36-081-10 (Brynes)
& APN 36-081-08 (Carbone)

The case studies for the combination of these three parcels revealed the difficulties and did not result in firmly establishing the street layout, but instead outlined the principles for the street frontage and interconnection of new development. In all cases, it was established that maintaining the existing open space at the entry to town was desirable.

CASE STUDY ONE

This case study shows similar shaped paired buildings which have pedestrian connections between buildings to small dispersed parking lots. However, the existing property lines would not permit this to be realized.

CASE STUDY TWO

This case study looked at aggregating and locating most of the parking required for all three parcels along the highway frontage of the Vintage property. This allowed creating a secondary street defined with pedestrian-oriented buildings which was connected to Washington Street.

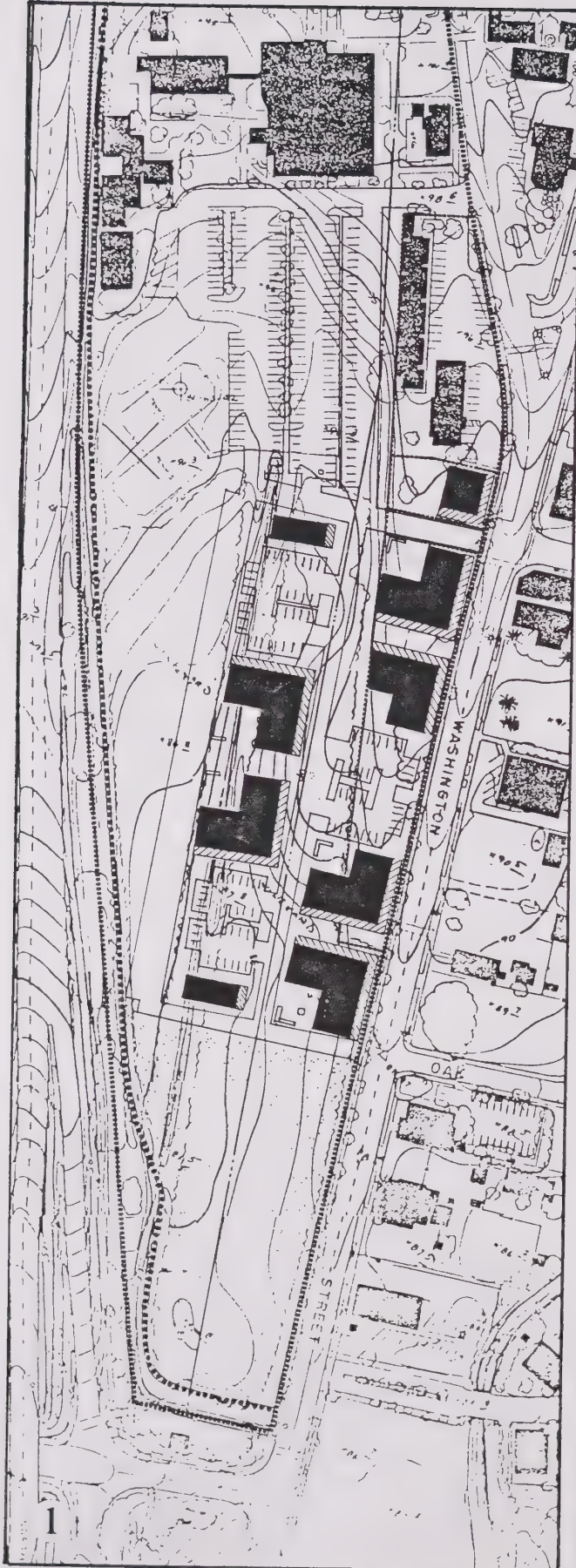
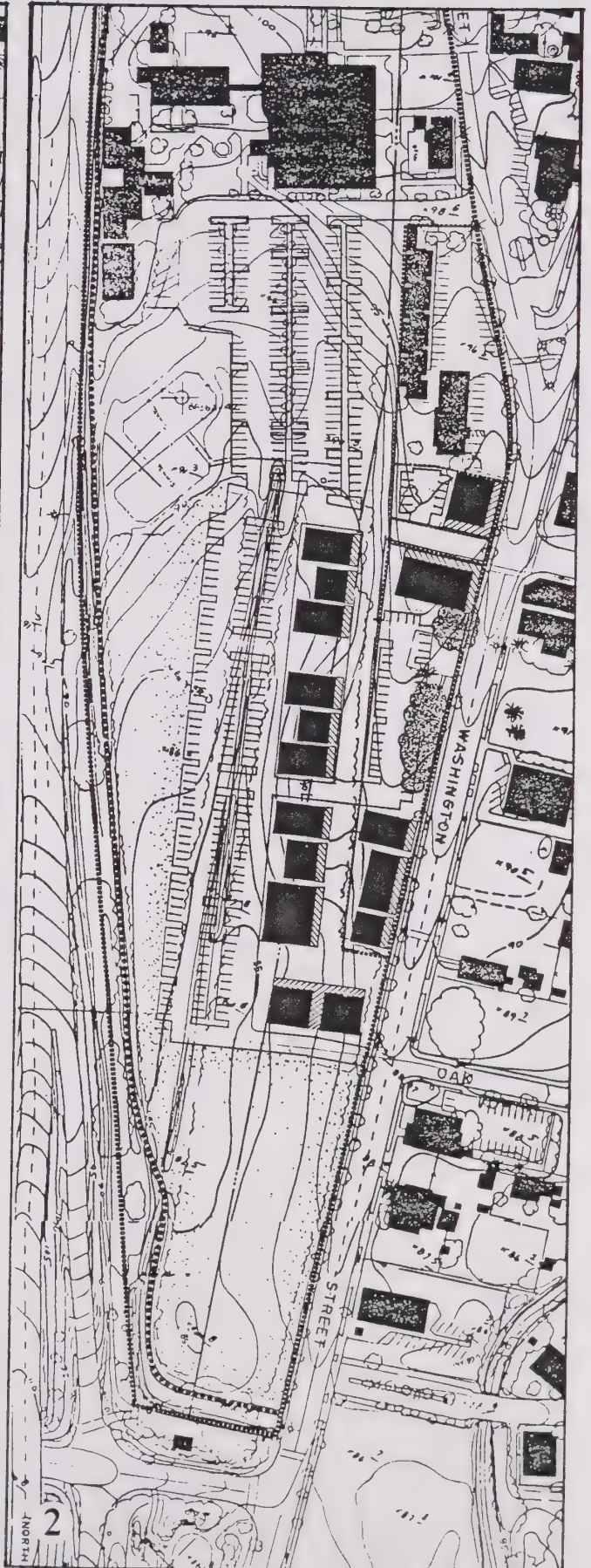


Figure IV.7



Vintage Partners, Brynes, Carbone

B. CROSS SECTIONS OF EXISTING STREETS

Some of the streets of Yountville have a distinctly rural character, while others are more suburban in nature. For the revision to the General Plan, Zoning Ordinance and new Design Document, the dimensions of existing streets were studied. The recorded dimensions assisted in determining the dimensions for proposed new streets which will be consistent with the rural character. They are appear in this document for reference only, and include the following: 1 **Washington Street** at Madison Street; 2 **Washington Street** near Yount Street; 3 **Washington Street** at Oak Circle; 4 **Jefferson Street** at Madison Street; 5 **Yount Street** at Madison Street; 6 **Creek Street**; and 7 **Vista Drive**.

B.1. WASHINGTON STREET AT MADISON STREET

Washington Street is the primary commercial street in Yountville, and is characterized by one and two-story buildings facing the street. At this portion of Washington Street, however, the street frontages vary significantly. The buildings on the east side are not set back from the street edge. The newer buildings on the west side have substantial setbacks, planted areas, and sidewalks. The 50 foot wide paved area includes two travel lanes and parallel parking on both sides of the street.

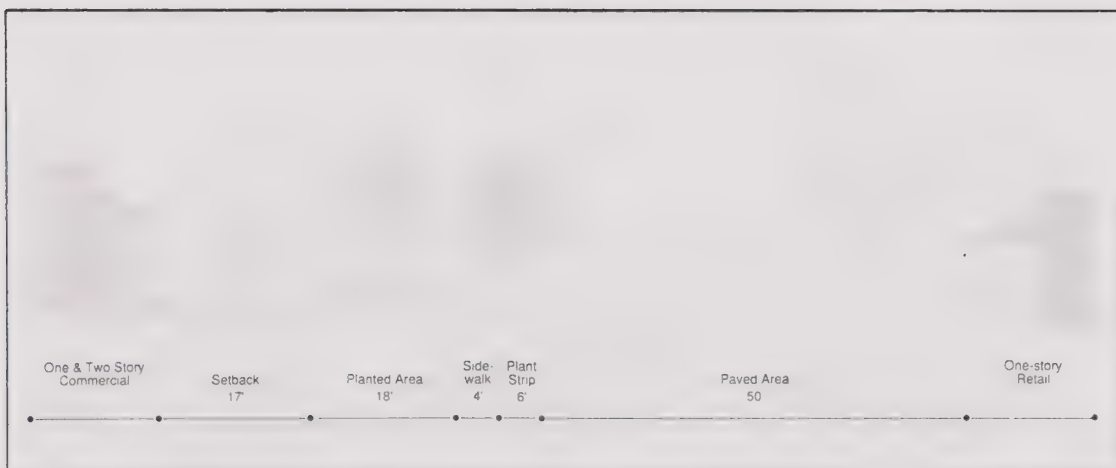


Figure IV.8

Cross Section of Washington Street

B.4. JEFFERSON STREET AT MADISON STREET

Jefferson Street is a primary north-south residential street in Old Town. The width and character are consistently rural in nature. The 36 foot wide paved area accommodates two travel lanes, and parallel on-street parking on both sides. Eight foot wide gravel shoulders flank both sides of the street, accommodate street trees, and provide a walkable path for residents. The single-family houses are predominately one story and have generous front yards set back from the street. Hedges and fences give definition to the street edge and a sense of privacy to the front yards. The dimensions and character of Jefferson Street served as a model for the proposed streets in Chapter I.

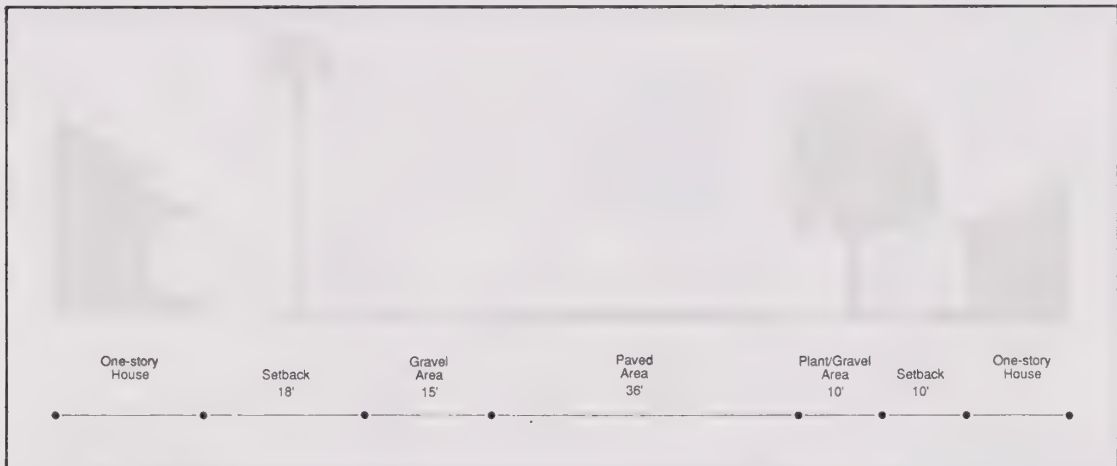


Figure IV.11

Cross Section of Jefferson Street

B.5. YOUNT STREET AT MADISON STREET

The street width along Yount Street is consistent, however, the street is ill-defined and there are many different types of buildings and uses. Near the intersection at Madison Street, buildings are set back at least 15 feet from the street. Some have front yards along the street frontage, and others have parking lots. The buildings are one and two-story, and either single-family houses or apartments. The 40 feet wide paved area includes asphalt shoulders used for on-street parking and bicyclists. There are few street trees, and no planting areas or gravel shoulders on either side.



Figure IV.12

Cross Section of Yount Street

B . 6 .

CREEK STREET

Creek Street is narrow residential street consisting of a 25 foot wide paved area flanked by gravel shoulders that vary from three to eight feet wide, and have a few street trees and some areas for on-street parking. Fences and hedges at the edge of the gravel shoulders delineate the boundary of the street with the front yards. However, in some places there are no setbacks between buildings and the gravel shoulders. The buildings are a mix of one and two-story residences, and residential-scaled commercial uses. The dimensions and character of Creek Street reinforce the rural character of Yountville, and served as the model for the residential alley defined in Chapter I.

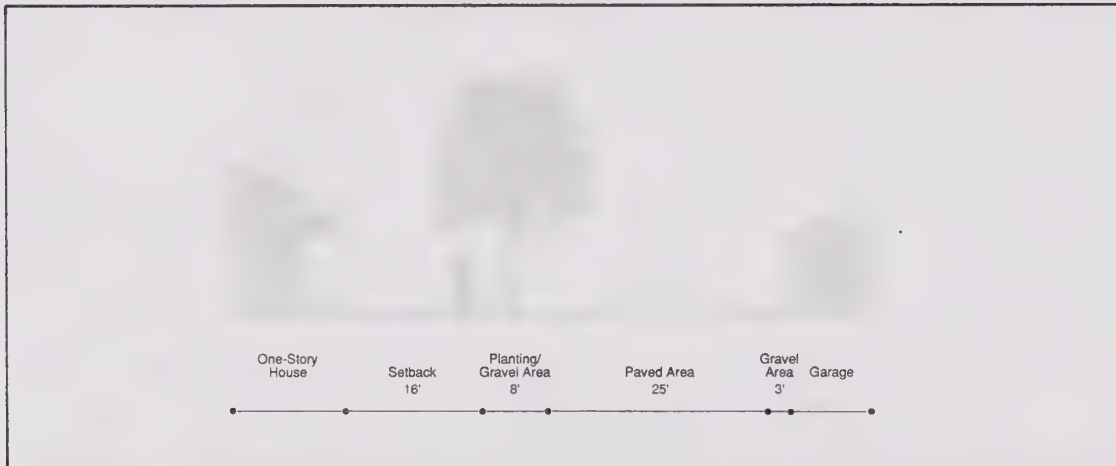


Figure IV.13

Cross Section of Creek Street

B . 7 .

VISTA DRIVE

Vista Drive is typical of the streets in newer subdivisions built in the eastern portion of town. The 60 foot wide right-of-way consists of a 40 foot wide paved area including curbs and gutters, flanked on both sides by a four foot wide planting strip of grass and trees, and six foot wide sidewalks. The buildings are predominately single-story residences, and setback at least 10 feet from the street. The width and character of this street type is damaging to the rural character of Yountville, and will be disallowed in new development following the adoption of the General Plan, and this document.

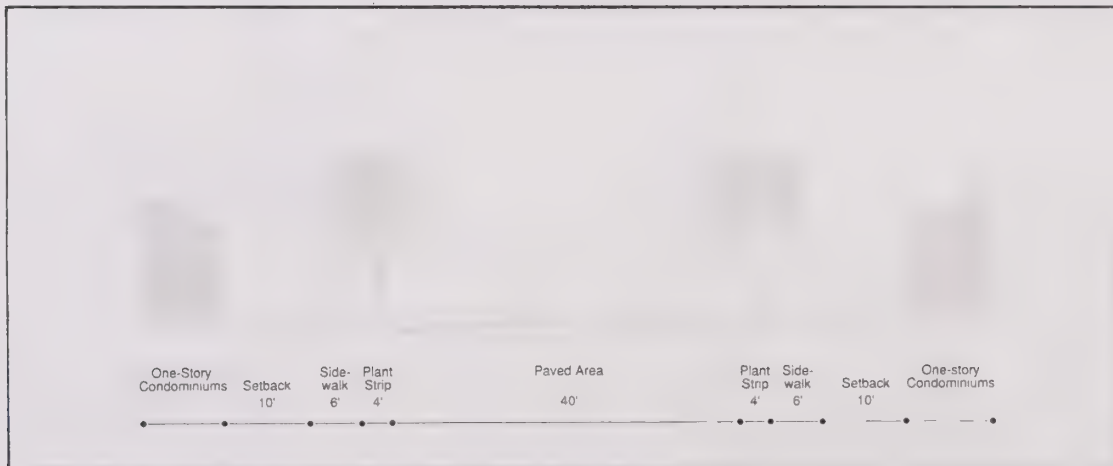


Figure IV.14

Cross Section of Vista Drive

The purpose of this study was to investigate the relationship between the quality of the school environment and the quality of the teaching and learning process. The study was conducted in a large urban school district in the United States. The study was a quantitative study and used a survey design. The study was conducted in a large urban school district in the United States. The study was a quantitative study and used a survey design. The study was conducted in a large urban school district in the United States. The study was a quantitative study and used a survey design.

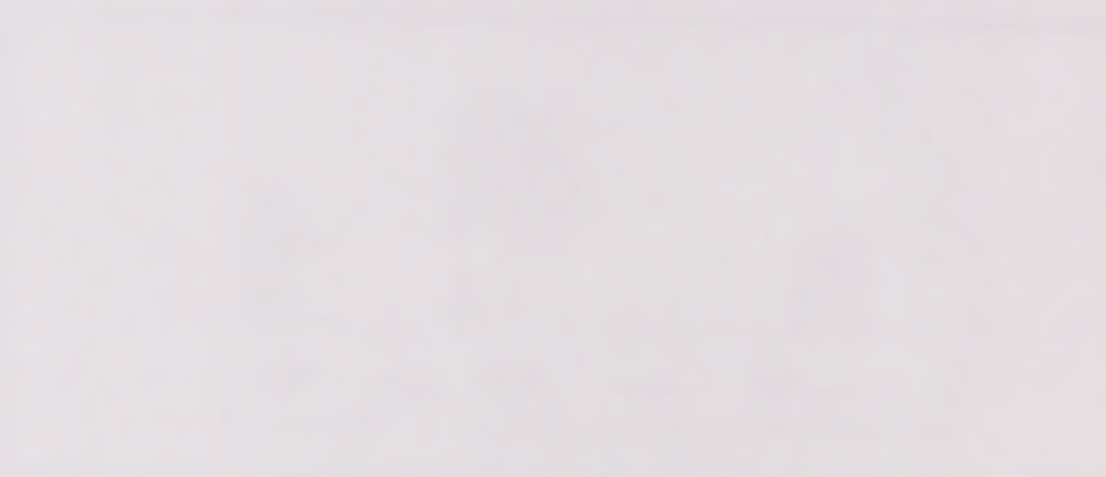


Figure 1: Quality of the School Environment and Quality of the Teaching and Learning Process

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Figure 2: Quality of the School Environment and Quality of the Teaching and Learning Process

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